

BREA INDUSTRIAL SPECIFIC PLAN

Specific Plan No. PR 98-32

City of Brea

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1.0 INTRODUCTION AND SUMMARY

The Brea Industrial Specific Plan shall serve as a guideline for the character, design and standards for the development of 37 acres located within the City of Brea, Orange County, California. The Brea Industrial Specific Plan provides for an industrial park development consisting of office, warehouse and light industrial uses. The proposed development shall replace vacant land and a small orange grove located on the southwest portion of the property. The project site is currently surrounded by existing development. The Brea Industrial Specific Plan will establish the development regulations, policies and site development standards to implement the Land Use Plan as ultimately approved by the Brea City Council. The Land Use Plan is consistent with the current General Plan and Zoning designations of the property. This Specific Plan is regulatory, adopted by ordinance, and upon adoption will continue to provide consistency with the City of Brea General Plan.

The project site is located in the southwest section of the City of Brea, approximately 2 miles west of the SR-57 Freeway. The 37-acre site is generally bounded by Imperial Highway on the south, Puente Street on the west, Southern Pacific Railroad right-of-way on the north, and Arovista Avenue and existing light industrial development on the east. The site is located within an urbanized area of northeast Orange County and is surrounded by retail commercial and residential uses to the south, and light industrial, office and warehouse uses to the north, east and west.

The Brea Industrial Specific Plan provides for a industrial, office and warehouse/distribution development under the Specific Plan designation contained in the adopted City of Brea General Plan. The Specific Plan contains development controls to provide the City of Brea with assurances that the site area will be developed as planned. The specific plan is intended to provide a degree of flexibility to allow future development to respond to changes in society and the economic marketplace, which will occur over the build out period of the project. The Specific Plan establishes the design and development policies applicable to development within the Specific Plan area. The designs encompass project planning design, landscaping, architecture, signage, lighting and project character.

The proposed project consists of a specific plan which includes industrial, office and warehouse/distribution uses. By City of Brea Ordinance, a maximum of 806,078 square feet of lot coverage is allowed within the 37-acre boundary of the Specific Plan area. In addition to the industrial, office and warehouse/distribution structures, the project will also provide associated parking and the option to connect to an existing railroad spur adjacent to the northern boundary of the project. The land use theme is anticipated to be industrial and warehouse/distribution oriented with associated office uses. Access to the site will be provided from Imperial Highway, Puente Street and Arovista Avenue. In addition, Mariner Street is proposed to extend from its existing terminus east of the site to connect with Puente Street to the west.

The authority for preparation of specific plans is found in the California Government Code, Section 65450, et seq. The law allows for the preparation of specific plans based on the General Plan, as may be required for the systematic execution of the General Plan and further allows for their review and adoption.

2.0 PROJECT LOCATION AND DESCRIPTION

2.1 PURPOSE AND INTENT

The purpose of the Brea Industrial Specific Plan is to allow for the development of 619,250 square feet of industrial, office and warehouse/distribution uses within the city limits of Brea. The planned development will replace the existing vacant land and citrus orchards by means of a General Plan Amendment and Zone Change to be implemented in conjunction with this Specific Plan.

The Specific Plan document implements a City's General Plan through the provision of a more detailed version of the General Plan for a specified area. The Specific Plan articulates planning considerations and imposes regulations and/or controls for the project area. Responsible planning is ensured through the adoption of a development control mechanism that reflects thorough land use planning. A suitable development control mechanism is the Specific Plan which, when adopted by the City of Brea, serves both a planning function and a regulatory function.

The adoption of the Brea Industrial Specific Plan will establish the type, location, intensity and character of development and the required infrastructure in order for development to occur. The Specific Plan will also guide development in order to respond to the physical constraints and environmental sensitivities of the site, coordinate the land uses and provide plans for grading, circulation and drainage. To ensure that the design criteria are adhered to, detailed designs are incorporated herein.

2.2 PROJECT LOCATION AND SETTING

The City of Brea is situated in North Orange County, California, adjacent to both San Bernardino and Los Angeles Counties. Brea is approximately 30 miles southeast of downtown Los Angeles by freeway as shown on Figure 2-1 (Regional Location Map). The Brea Industrial Specific Plan is located in the southwest section of the City, approximately 2 miles west of Interstate 57. The 37-acre site is a square shaped parcel surrounded on all sides by existing development. As shown in Figure 2-2 (Vicinity Map), the site is bounded by a Southern Pacific Railroad line to the north, existing light industrial development and Arovista Avenue to the east, Imperial Highway to the south, and Puente Street to the west. As shown in Figure 2-3 (Aerial Photograph/Surrounding Land Uses) the current land use of the site consists of some existing citrus tree orchards in the southwest and northwest corners, vacant land on the entire western portion, and existing paving on the central portion of the site. There are two existing access points to the property from Puente Street and an existing railroad spur entering the property from the northwest corner. The site is surrounded by industrial development to the north, east and west, and residential and retail development to the south.

A small piece of property located on the northwest corner of Imperial Highway and Arovista Avenue is not a part of the proposed project. This small parcel contains an existing art sculpture and associated landscaping. Maintenance of this parcel is not the responsibility of the project developer.

2.2.1 Site History

Historically, the subject property was utilized for agricultural purposes. In 1959, Petrolite Company purchased the 37 acre site and constructed a chemical plant on approximately 12 acres of the northwest portion. The primary use was manufacturing of specialty chemicals for oil fields and administrative support. An additional 12 acres of the site remained in agricultural use, with the balance being undeveloped and vacant. In 1987, Petrolite Company abandoned the facility and in 1988 dismantled the chemical plant.

Since 1988, the subject property has been involved in efforts to remove any hazardous materials located in the soil and groundwater from the previous chemical plant use. Remediation has been completed and the property has received an official closure letter from the California Regional Water Quality Control Board dated November 9, 1998. In addition, agricultural uses involving citrus orchards continue to exist on approximately 12 acres of the site. There are no structures currently existing on the site.

2.2.2 Environmental Setting

The Specific Plan area is generally flat with fifteen (15) feet of elevation differences from the extreme high to extreme low. The nearest significant drainage feature is a south and west flowing flood control channel, which is located one quarter mile west of the site. The Brea Canyon Channel is located approximately one half mile east of the site and drains runoff from the hills to the north and east.

Information regarding soil type, groundwater levels, earthquake faults in the region and other pertinent environmental conditions are evaluated in the project's Environmental Impact Report (EIR) as required by the California Environmental Quality Act (CEQA).

REGIONAL LOCATION MAP

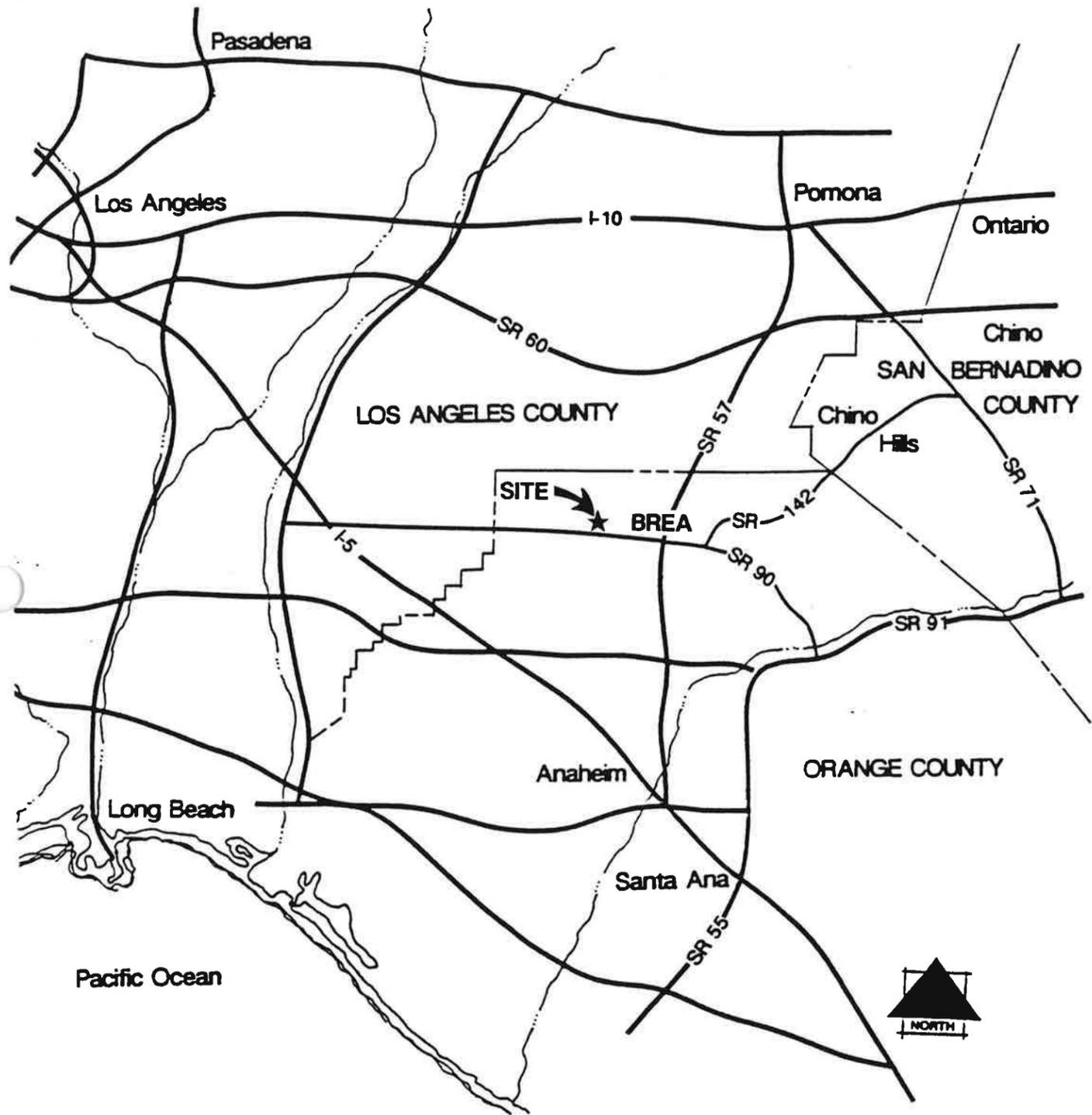


FIGURE 2-1



The Kelth Companies
Inland Empire Division

VICINITY MAP

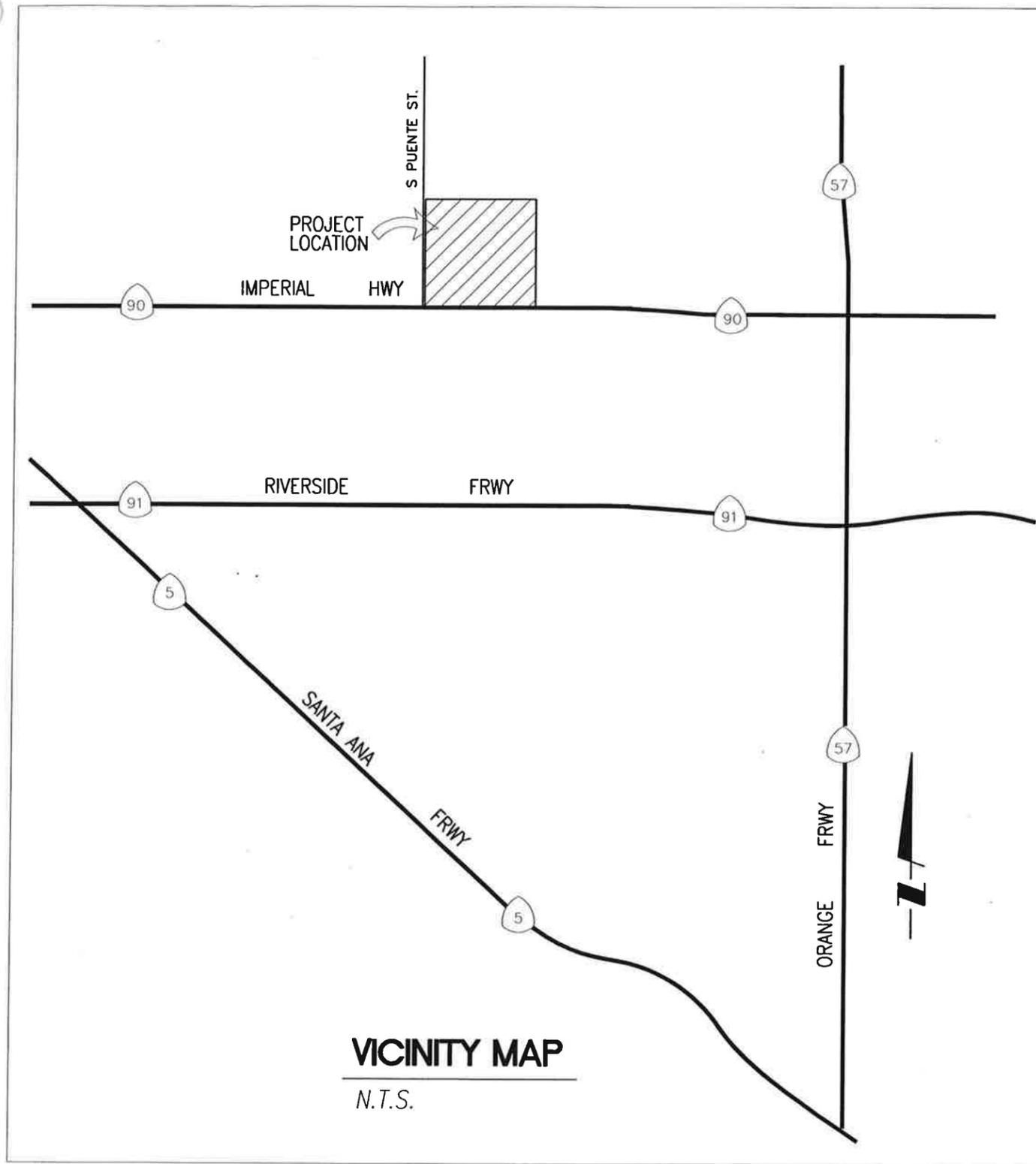
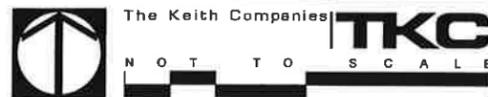


FIGURE 2-2



AERIAL PHOTOGRAPH/ SURROUNDING LAND USES



FIGURE 2-3

2.3 PROJECT OBJECTIVES

The Specific Plan contains all components required by State law, as well as other components, design concepts, guidelines, and standards desired by the City to implement the General Plan. The objectives of this Specific Plan are to:

- Provide a comprehensive land use plan that designates the distribution, location, and extent of land uses, collector roadways, and public facilities with the Specific Plan area;
- Promote development consisting of the highest and best use of the site subsequent to removing the existing citrus orchards and impervious surfaces.
- Increase the availability of goods and services to Brea area residents.
- Provide additional employment opportunities to Brea area residents.
- Provide for the aesthetic restoration / enhancement of the project site through removal of the current agricultural use while allowing the project proponent to redevelop the site with a more appropriate land use consistent with surrounding development. Aesthetic improvements will consist of landscaping and architectural design features consistent with the Conceptual Landscape Program and Design Guidelines, respectively.
- Verify prior to development of the site that all remediation requirements for the former chemical plant use of the property have been met and appropriate closure documentation is obtained from relevant agencies.
- Establish Design Guidelines and development regulations and procedures to control future project improvements and provide appropriate architectural themes for the project area.
- Develop a design that ensures the public's health, safety and welfare.
- Provide access patterns which minimize use conflicts.
- Plan and construct capital improvements for water, sewer, drainage and road facilities to adequately service the project.

2.4 AUTHORITY AND REQUIREMENTS**2.4.1 Authority for Specific Plans**

The authority to prepare, adopt and implement Specific Plans is granted to the City of Brea by the California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457). As with General Plans, the Planning Commission must hold a public hearing before it can recommend adoption of a Specific Plan. The City Council may adopt a Specific Plan by either ordinance or resolution.

2.4.2 Requirements for Specific Plans

California Government Code Section 65451 sets forth the requirements for Specific Plans as follows:

A Specific Plan shall include a text and a diagram or diagrams which specify all of the following in detail:

- 1) *The distribution, location and extent of the uses of land, including open space, with the area covered by the plan;*
- 2) *The proposed distribution, location and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located with the area covered by the plan and needed to support the land uses described in the plan;*
- 3) *Standards and criteria by which improvements will proceed, and standards for the conservation, development and utilization of natural resources, where applicable, and;*

A program of implementation measures including regulations, programs, public works projects and the financing measures necessary to carry out items 1, 2 and 3 above.

A statement of the relationship of the Specific Plan to the General Plan.

The Specific Plan is a regulatory plan which will serve as the zoning for the subject property. Proposed development plans or agreements, tentative tracts or parcel maps, and any other development approval must be consistent with the Specific Plan. Projects which are found consistent with the Specific Plan will be deemed consistent with the City's General Plan.

2.5 RELATIONSHIPS TO THE CALIFORNIA ENVIRONMENTAL QUALITY ACT

Pursuant to the California Environmental Quality Act (CEQA) Guidelines, the City of Brea prepared an "Initial Study" which determined that the Brea Industrial Specific Plan may result in significant environmental impacts and therefore required preparation of an Environmental Impact Report (EIR). Upon submittal of this Specific Plan document and appropriate application materials to the City of Brea, a third party consultant to the City will begin preparation of the EIR. The EIR will evaluate the potential environmental impacts associated with development of the proposed project and will identify appropriate mitigation measures for each significant impact. All mitigation measures included in the Final EIR shall be made specific conditions of approval to the Brea Industrial Specific Plan.

The Final EIR certified for the Specific Plan will be intended to apply to all subsequent development projects processed in conformance with this Specific Plan. All future development projects for the project site will be reviewed with the Specific Plan and EIR to determine whether additional environmental documentation must be prepared pursuant to Section 15162, 15168, and 15182 of the CEQA Guidelines.

3.0 DEVELOPMENT PLAN

3.1 PURPOSE AND INTENT

The purpose of the Brea Industrial Specific Plan is to assure a high quality industrial project through the development regulations and design guidelines contained herein. The intent of the Specific Plan and its regulations is to provide development criteria to guide the developer(s) and the City of Brea in the design of new industrial descriptive text and exhibits which will set forth the basis for the overall master development of the project site. The components provided in this section are further defined for each planning area through the development regulations contained in Section 4.0, Development Regulations and Procedures.

The development plan has been derived from several project alternatives and input from studies prepared by the applicant's civil engineer, traffic engineer, land planners, architects and involvement by the City of Brea Development Services Department. As a result, the plan addresses development-related issues in the form of proposed physical improvements, guidelines for future development, technical data, implementation procedures and regulations.

3.2 LAND USE PLAN

The Brea Industrial Specific plan proposes approximately 37 acres of industrial land use, which is inclusive of a mix of office, warehousing and distribution activities. The project is separated into 7 individual planning areas for development of the proposed uses. In accordance with the City of Brea Zoning Ordinance, the development regulations for each planning area will be based on one of two industrial land use designations; Planning Areas 1, 2 and 3 will include General Industrial land uses and Planning Areas 4, 5, 6 and 7 will include Light Industrial land uses. Refer to Section 4.0 of the Specific Plan for further delineation of the development regulations and procedures for each industrial land use designation. The extension of Mariner Street from its current terminus at Arovista Avenue west through the project site will serve as the division line between the light and general industrial designations (See Figure 3-1, Conceptual Land Use Plan). The Mariner Street Extension will provide primary access to the project from Puente Street on the west and Arovista Avenue on the east. Access to the project will be provided from the south by "A" Street off of Imperial Highway. As depicted in the Conceptual Land Use Plan, Figure 3-1, and summarized in Table 3-1, the land use categories identified for the Brea Industrial Specific Plan include:

- General Industrial (Planning Areas 1, 2 and 3): provides for 17.67 acres (769,705 square feet) of General Industrial land uses as identified in Section 4.4, General Industrial Zone. A maximum of 330,973 square feet (43 percent) of lot coverage is permitted within this land use designation. Allowable maximum lot coverage for each individual Planning Area is further delineated in Table 3-1. Within the General Industrial portion of the project, the Specific Plan shall allow for minor modifications to the proposed location, number, and size of the Planning Areas/Buildings, provided that the maximum allowable lot coverage of 330,973 square feet is not exceeded.
- Light Industrial (Planning Areas 4, 5, 6 and 7): provides for 17.46 acres (760,558 square feet) of Light Industrial land uses as identified in Section 4.3, Light Industrial Zone. A maximum of 289,011 square feet (38 percent) of lot coverage is permitted within this land use designation. Allowable maximum lot coverage for each Planning Area is further delineated in Table 3-1. Within the Light Industrial portion of the project, the Specific Plan shall allow for minor modifications to the proposed location, number, and size of the Planning Areas/Buildings, provided that the maximum allowable lot coverage of 289,011 square feet is not exceeded.

The total project is designed to provide a range of industrial land use opportunities including office, warehousing and distribution activities within nine (9) primary buildings. The three (3) primary buildings located north of the Mariner Street Extension will generally contain land uses that are regulated under Section 4.4, General Industrial Zone. The six (6) primary buildings located south of the Mariner Street Extension will contain land uses regulated under Section 4.3, Light Industrial Zone. The General Industrial Zone includes certain uses permitted within the Light Industrial Zoning designation (see Section 4.4.1).

CONCEPTUAL LAND USE PLAN



**BREA INDUSTRIAL
SPECIFIC PLAN**

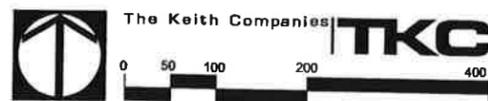


FIGURE 3-1

Table 3-1
LAND USE PLAN SUMMARY

Planning Area	Specific Plan Designation	Planning Area Acreage (Ac.)	Planning Area Square Feet (S.F.)	Max. Allowable Lot Coverage (S.F.)
1	General Industrial	5.29	230,432	99,086
2	General Industrial	5.60	243,936	104,892
3	General Industrial	6.78	295,337	126,995
<i>Subtotal:</i>		<i>17.67</i>	<i>769,705</i>	<i>330,973¹</i>
4	Light Industrial	4.53	197,327	74,984
5	Light Industrial	3.90	169,884	64,556
6	Light Industrial	4.59	199,940	75,977
7	Light Industrial	4.44	193,407	73,494
<i>Subtotal:</i>		<i>17.46</i>	<i>760,558</i>	<i>289,011²</i>
Circulation		1.87	-	-
TOTAL:		37.00	1,530,263	619,984

¹ Within the Light Industrial portion of the project, the Specific Plan shall allow for minor modifications to the proposed location, number, and size of the Planning Areas/Buildings, provided that the maximum allowable lot coverage of 289,011 square feet is not exceeded.

² Within the General Industrial portion of the project, the Specific Plan shall allow for minor modifications to the proposed location, number, and size of the Planning Areas/Buildings, provided that the maximum allowable lot coverage of 330,973 square feet is not exceeded.

3.2.1 Planning Areas

Planning Area 1 is located in the northwest corner of the project site, east of Puente Street. Planning Area 1 is regulated by Section 4.4 (General Industrial Zone) and consists of approximately 5.3 acres (230,432 square feet). Based on a maximum allowable lot coverage of 43 percent, the maximum floor area permitted within this Planning Area is 99,086 square feet. Access to Planning Area 1 will be provided from Puente Street and the Mariner Street Extension. Actual design features for Planning Area 1 will be determined by the Precise Development Plan. A conceptual site plan for Planning Area 1 is shown on Figure 3-1A.

Planning Area 2 is located on the north side of the Mariner Street Extension, between Planning Areas 1 and 3. Planning Area 2 is regulated by Section 4.4 (General Industrial Zone) and consists of approximately 5.6 acres (243,936 square feet). Based on a maximum allowable lot coverage of 43 percent, the maximum floor area permitted within this Planning Area is 104,892 square feet. Access to Planning Area 2 will be provided on the south side from the Mariner Street Extension. Additional access to Planning Area 2 is provided from the internal parking lot circulation from Planning Areas 1 and 3. Actual design features for Planning Area 2 will be determined by the Precise Development Plan. A conceptual site plan for Planning Area 2 is shown on Figure 3-1A.

Planning Area 3 is located in the northeast corner of the project site, on the north side of the Mariner Street Extension. Planning Area 3 is regulated by Section 4.4 (General Industrial Zone) and consists of approximately 6.8 acres (295,337 square feet). Based on a maximum allowable lot coverage of 43 percent, the maximum floor area permitted within this Planning Area is 126,995 square feet. Access to Planning Area 3 will be provided from the Mariner Street Extension. Actual design features for Planning Area 3 will be determined by the Precise Development Plan. A conceptual site plan for Planning Area 3 is shown on Figure 3-1A.

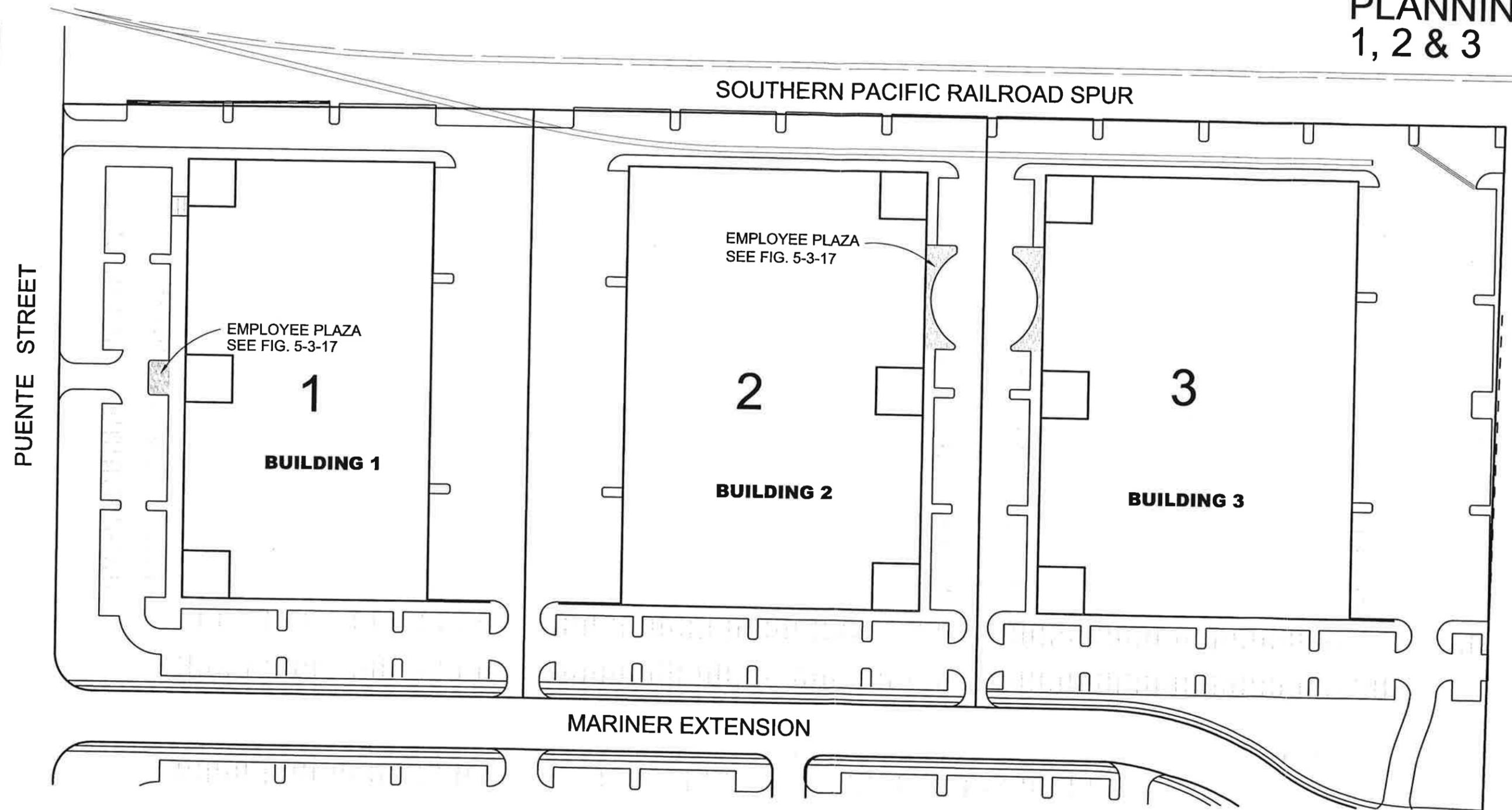
Planning Area 4 is located on the southeast corner of the intersection of Puente Street and the Mariner Street Extension. Planning Area 4 is regulated by Section 4.3 (Light Industrial Zone) and consists of approximately 4.53 acres (197,327 square feet). Based on a maximum allowable lot coverage of 38 percent, the maximum floor area permitted within this Planning Area is 74,984 square feet. Access to Planning Area 4 will be provided from the Mariner Street Extension on the north, Puente Street on the west and "A" Street on the east. Actual design features for Planning Area 4 will be determined by the Precise Development Plan. A conceptual site plan for Planning Area 4 is shown on Figure 3-1B.

Planning Area 5 is located on the southwest corner of the intersection of the Mariner Street Extension and Arovista Avenue. Planning Area 5 is regulated by Section 4.3 (Light Industrial Zone) and consists of approximately 3.90 acres (169,884 square feet). Based on a maximum allowable lot coverage of 38 percent, the maximum floor area permitted within this Planning Area is 64,556 square feet. Access to Planning Area 5 will be provided from the Mariner Street Extension on the north, "A" Street on the west and Arovista Avenue on the east. Actual design features for Planning Area 5 will be determined by the Precise Development Plan. A conceptual site plan for Planning Area 5 is shown on Figure 3-1B.

Planning Area 6 is located in the southwest corner of the project site, on the east side of Puente Street and north of Imperial Highway. Planning Area 6 is regulated by Section 4.3 (Light Industrial Zone) and consists of approximately 4.59 acres (199,940 square feet). Based on a maximum allowable lot coverage of 38 percent, the maximum floor area permitted within this Planning Area is 75,977 square feet. Access to Planning Area 6 will be provided from Puente Street on the west and "A" Street on the east. Actual design features for Planning Area 6 will be determined by the Precise Development Plan. A conceptual site plan for Planning Area 6 is shown on Figure 3-1B.

Planning Area 7 is located in the southeast corner of the project site, on the west side of Arovista Avenue. Planning Area 7 is regulated by Section 4.3 (Light Industrial Zone) and consists of approximately 4.44 acres (193,407 square feet). Based on a maximum allowable lot coverage of 38 percent, the maximum floor area permitted within this Planning Area is 73,494 square feet. Access to Planning Area 7 will be provided from Arovista Avenue on the east and "A" Street on the west. Actual design features for Planning Area 7 will be determined by the Precise Development Plan. A conceptual site plan for Planning Area 7 is shown on Figure 3-1B.

CONCEPTUAL SITE PLANS PLANNING AREAS 1, 2 & 3



- PLANNING AREA 1**
 GENERAL INDUSTRIAL
 5.29 ACRES
 230,432 SQUARE FEET
 99,086 SQ. FT. MAXIMUM
 ALLOWABLE LOT COVERAGE
- PLANNING AREA 2**
 GENERAL INDUSTRIAL
 5.60 ACRES
 243,936 SQUARE FEET
 104,892 SQ. FT. MAXIMUM
 ALLOWABLE LOT COVERAGE
- PLANNING AREA 3**
 GENERAL INDUSTRIAL
 6.78 ACRES
 295,337 SQUARE FEET
 126,995 SQ. FT. MAXIMUM
 ALLOWABLE LOT COVERAGE

PUENTE STREET

SOUTHERN PACIFIC RAILROAD SPUR

MARINER EXTENSION

EMPLOYEE PLAZA
SEE FIG. 5-3-17

EMPLOYEE PLAZA
SEE FIG. 5-3-17

1
BUILDING 1

2
BUILDING 2

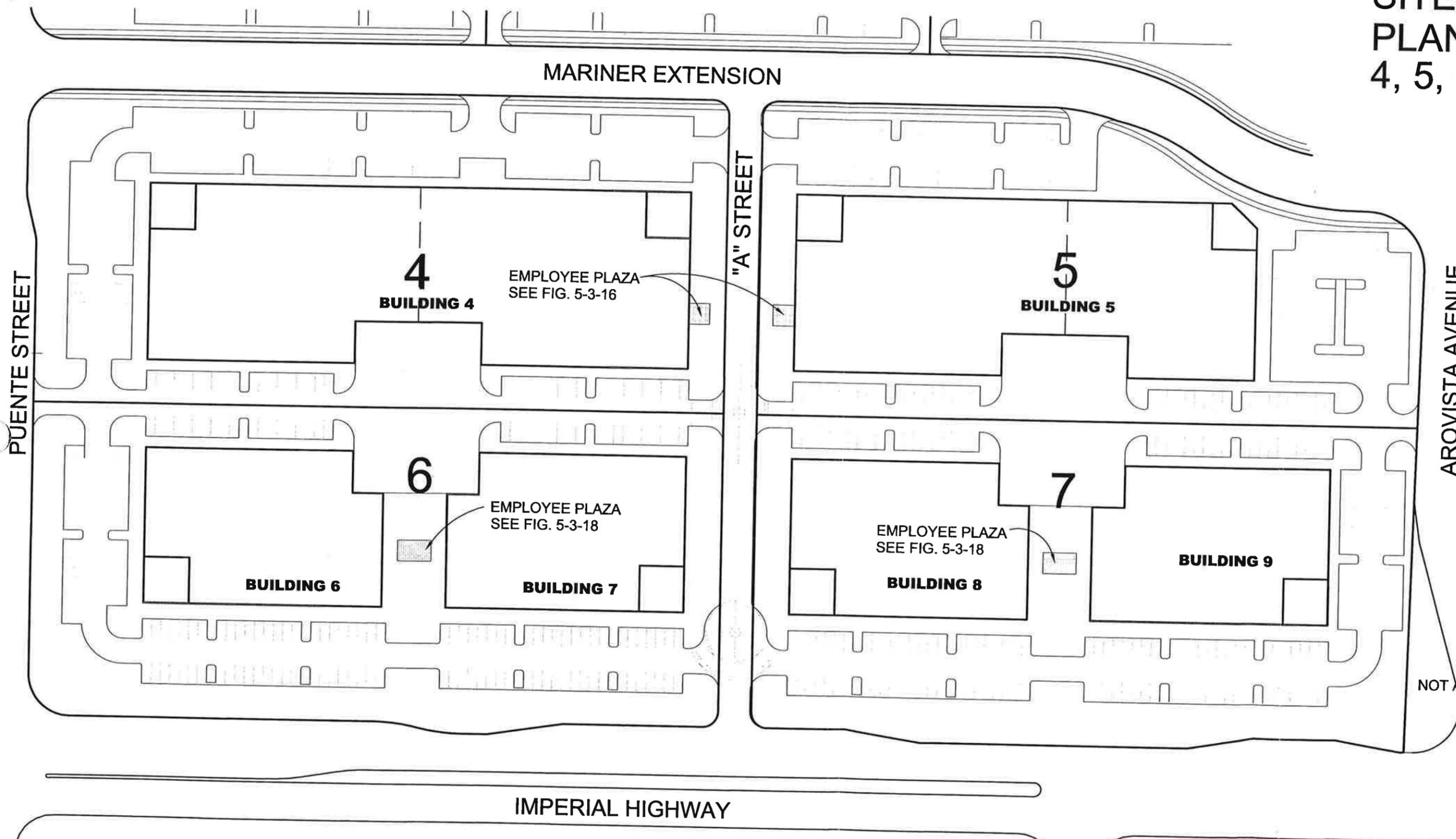
3
BUILDING 3

**BREA INDUSTRIAL
SPECIFIC PLAN**



FIGURE 3-1A

CONCEPTUAL SITE PLANS PLANNING AREAS 4, 5, 6 & 7



- PLANNING AREA 4
LIGHT INDUSTRIAL
4.53 ACRES
197,327 SQUARE FEET
74,984 SQ. FT. MAXIMUM
ALLOWABLE LOT COVERAGE
- PLANNING AREA 5
LIGHT INDUSTRIAL
3.90 ACRES
169,884 SQUARE FEET
64,556 SQ. FT. MAXIMUM
ALLOWABLE LOT COVERAGE
- PLANNING AREA 6
LIGHT INDUSTRIAL
4.59 ACRES
199,940 SQUARE FEET
75,977 SQ. FT. MAXIMUM
ALLOWABLE LOT COVERAGE
- PLANNING AREA 7
LIGHT INDUSTRIAL
4.44 ACRES
193,407 SQUARE FEET
73,494 SQ. FT. MAXIMUM
ALLOWABLE LOT COVERAGE

**BREA INDUSTRIAL
SPECIFIC PLAN**

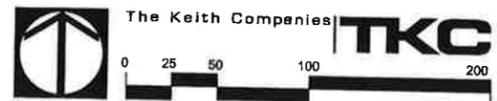


FIGURE 3-1B

3.3 GRADING PLAN

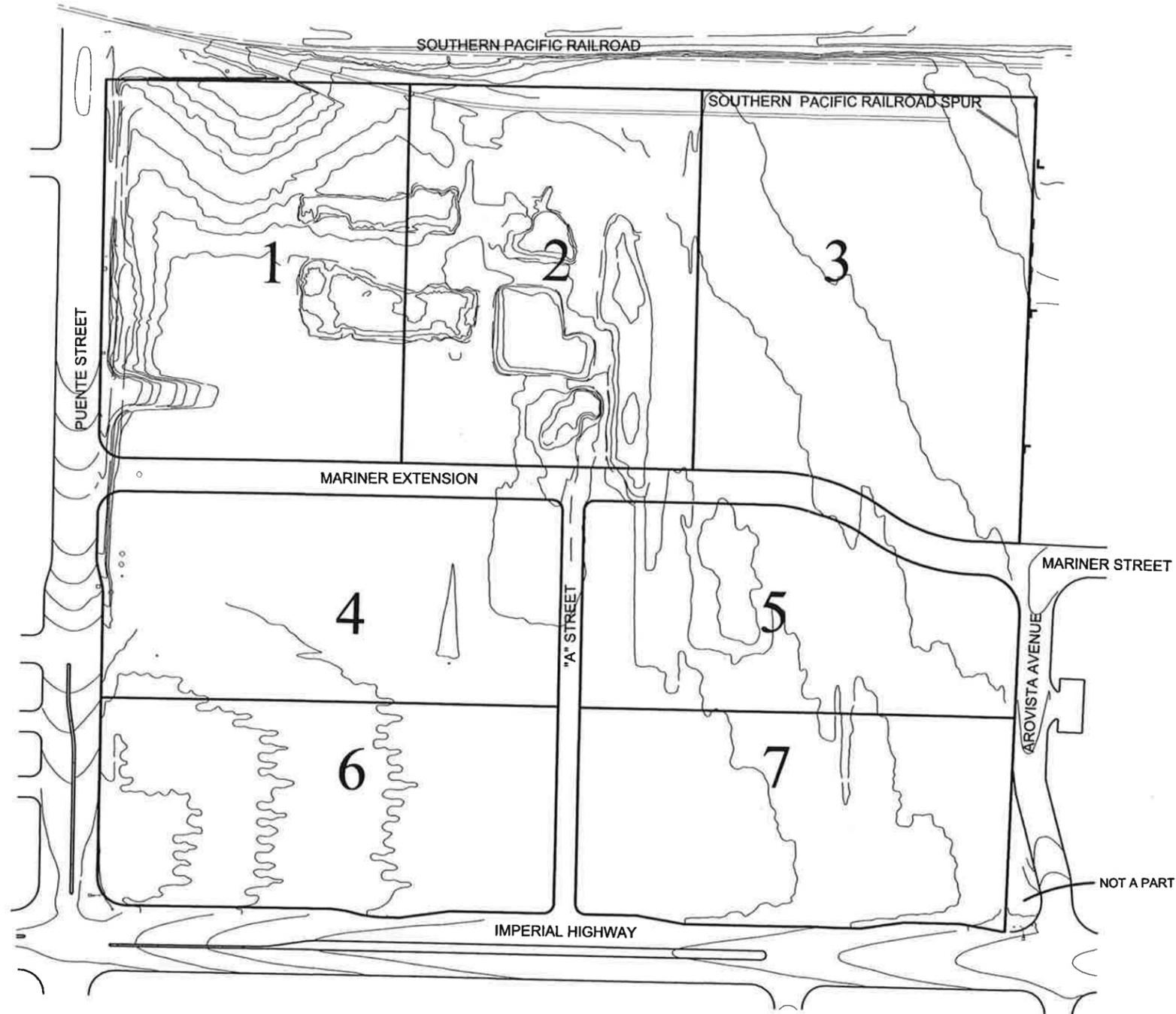
Figure 3-2 depicts the Conceptual Grading Plan for the Brea Industrial Specific Plan area located north of Imperial Highway and east of Puente Street. The Specific Plan area will not require a significant amount of grading. The site is generally flat, with fifteen feet of elevation difference from the extreme high to the extreme low point. The natural drainage of the site is to the southwest corner of the property. The intent of the Conceptual Grading Plan is to produce buildable parcels by a cutting of the higher areas to fill in the lower areas and achieve generally flat development pads for development. The proposed grading will result in ground elevations which are typically within 1 to 5 feet of the existing grade and will utilize the existing storm drain system. The building pad areas will be graded to approximately .5% with circulation and parking areas graded to an average of 2% to 3%.

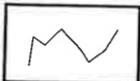
A slope easement, dedicated in favor of the City of Brea (Instrument No. 10482/66 O.R.), is located in the northwest corner of the property. This slope easement will be contained within a proposed landscaped area adjacent to proposed parking. It is anticipated that the easement will be deeded back to the property owner through the recordation of the proposed Tentative Parcel Map No. 98-180. Slope stabilization that takes place within the development plan will conform to the City of Brea grading ordinance and recommendations of a soils engineer.

The goals of the Conceptual Grading Plan are to mitigate flood hazards of the site, provide stable areas for proposed land uses, and maintain current sedimentation rates. To satisfy these goals, the grading concept, in conjunction with the Conceptual Storm Drain Plan (Figure 3-6), has been established and must be implemented so that the following occurs:

- A. All work shall be in accordance with the City of Brea Grading Ordinance and any special requirements of the grading permit.
- B. Subdrain installation shall be approved by the City of Brea and the project soils engineer and civil engineer. All subdrains will be maintained by the property owner or developer.
- C. All proposed drainage shall comply with the drainage standards administered by the City of Brea and Orange County Flood Control District (OCFCD).
- D. Landscaping shall consist of fire retardant and low fuel combustible plants approved by the City of Brea Fire Department.
- E. Graded areas on the permitted area perimeter must drain away from the face of slopes at the conclusion of each working day. Drainage shall be directed toward de-silting facilities.
- F. The permittee shall be responsible for, and shall take necessary precautions to, prevent public trespass onto areas where impounded water creates a hazardous condition.
- G. The project applicant shall prepare and file an NOI and NPDES permit containing a Best Management Practices Plan and Storm Water Pollution Prevention Plan with the Orange County Environmental Management Agency (OCEMA).

EXISTING TOPOGRAPHY



 Existing Topography

BREA INDUSTRIAL
SPECIFIC PLAN

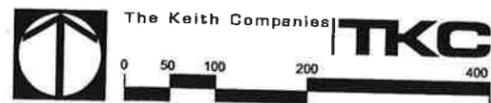
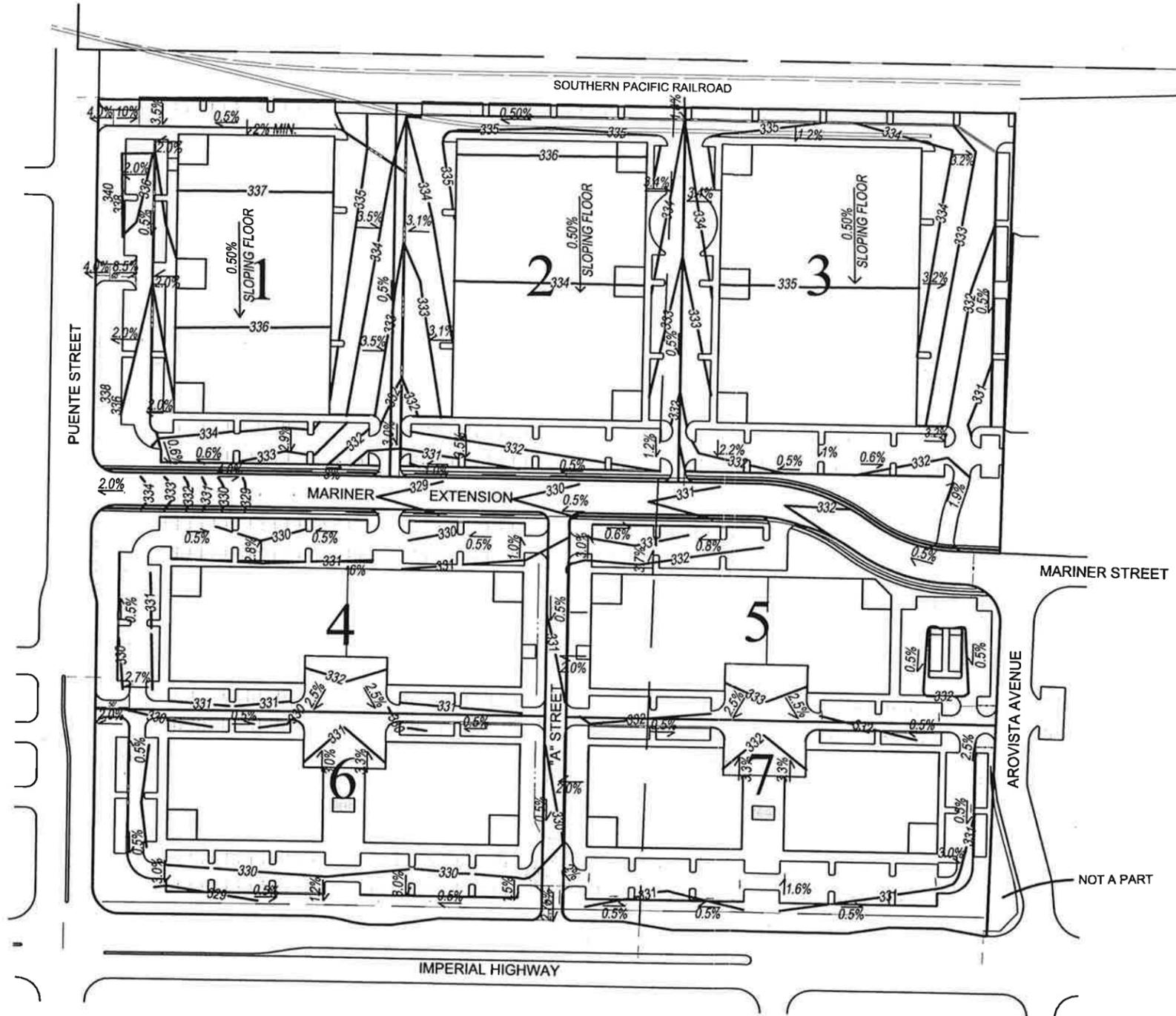


FIGURE 3-2A

CONCEPTUAL GRADING MASTER PLAN



— Proposed Grading

**BREA INDUSTRIAL
SPECIFIC PLAN**

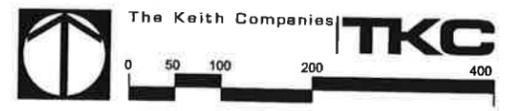


FIGURE 3-2B

3.4 CIRCULATION PLAN

The Brea Industrial Specific Plan establishes the layout of a circulation system and design standards for vehicular and pedestrian movement which is integrated with off-site arterials. The Conceptual Circulation Plan (Figure 3-3) responds to the needs of the development and takes into consideration drainage patterns and adjacent development. As shown on Figure 3-3, the proposed circulation network consists of an internal extension of Mariner Street from its existing terminus at the east boundary of the project site, west to form a T-intersection with Puente Street. An existing extension of Arovista Avenue along the eastern boundary of the project site will provide a connection between Mariner Street and Imperial Highway to the south. An additional access ("A" Street) shall serve as a primary "theme" entry into the central portion of the project from Imperial Highway located along the southern boundary of the project site. "A" Street will be privately maintained. A deceleration and acceleration lane will be provided on Imperial Highway adjacent to "A" Street. A right-turn lane will also be provided on Imperial Highway for traffic turning right onto Puente Street. A bus stop will also be provided on Imperial Highway between "A" Street and Arovista Avenue. A traffic signal shall be installed at the intersection of Mariner and Puente Streets when deemed necessary by the City Traffic Engineer.

The land use patterns are established in a manner to best utilize the existing adjacent arterials and the proposed internal extension of Mariner Street. The proposed Mariner Street Extension shall provide an internal circulation element which shall be primarily utilized by the project and shall reduce the project's impacts to the circulation on Imperial Highway and Puente Street. The street theme along Imperial Highway, Puente Street, Arovista Avenue and the Mariner Street Extension shall be designed in a manner which is consistent with the existing streetscenes along these roadways adjacent to the project site. The internal streetscene shall incorporate landscaped parkways to provide a continuation of those parkways which are existing and proposed along the Imperial Highway, Puente Street and Arovista Avenue.

3.4.1 Truck Traffic Circulation

A significant percentage of the traffic generated with this project will be in the form of truck traffic which is related to the warehousing and distribution operations. The majority of the truck traffic will be associated with the General Industrial portion of the project located in Planning Areas 1-3 north of the Mariner Street Extension. Concerns regarding the impact of the truck traffic on the residential land uses located south of Imperial Highway can be mitigated by enacting a restricted truck circulation route into, through and out of the project. The following circulation guidelines shall be implemented for the purposes of reducing impacts related to truck traffic associated with the project:

- A. Truck traffic shall be restricted from "A" Street for both project ingress and egress.
- B. Imperial Highway truck traffic arriving from the east shall be directed to enter the project via a right turn onto Arovista Avenue.
- C. Internal truck traffic accessing Mariner Street shall be directed east to Arovista Street and then south to Imperial Highway.

- D. The proposed project shall incorporate any approved conditions of approval associated with mitigation measures outlined in the Traffic Section of the project's EIR.
- E. Truck traffic shall avoid Lambert Road north of the project site during the hours of 8:00 p.m. to 6:00 a.m. Truck traffic on Lambert Road is prohibited during these hours.

The proposed Truck Traffic Circulation Guidelines, as depicted on Figure 3-3, will not completely eliminate the amount of truck traffic at the intersection of Imperial Highway and Puente Street. However, it will significantly reduce the amount generated by the Brea Industrial Specific Plan.

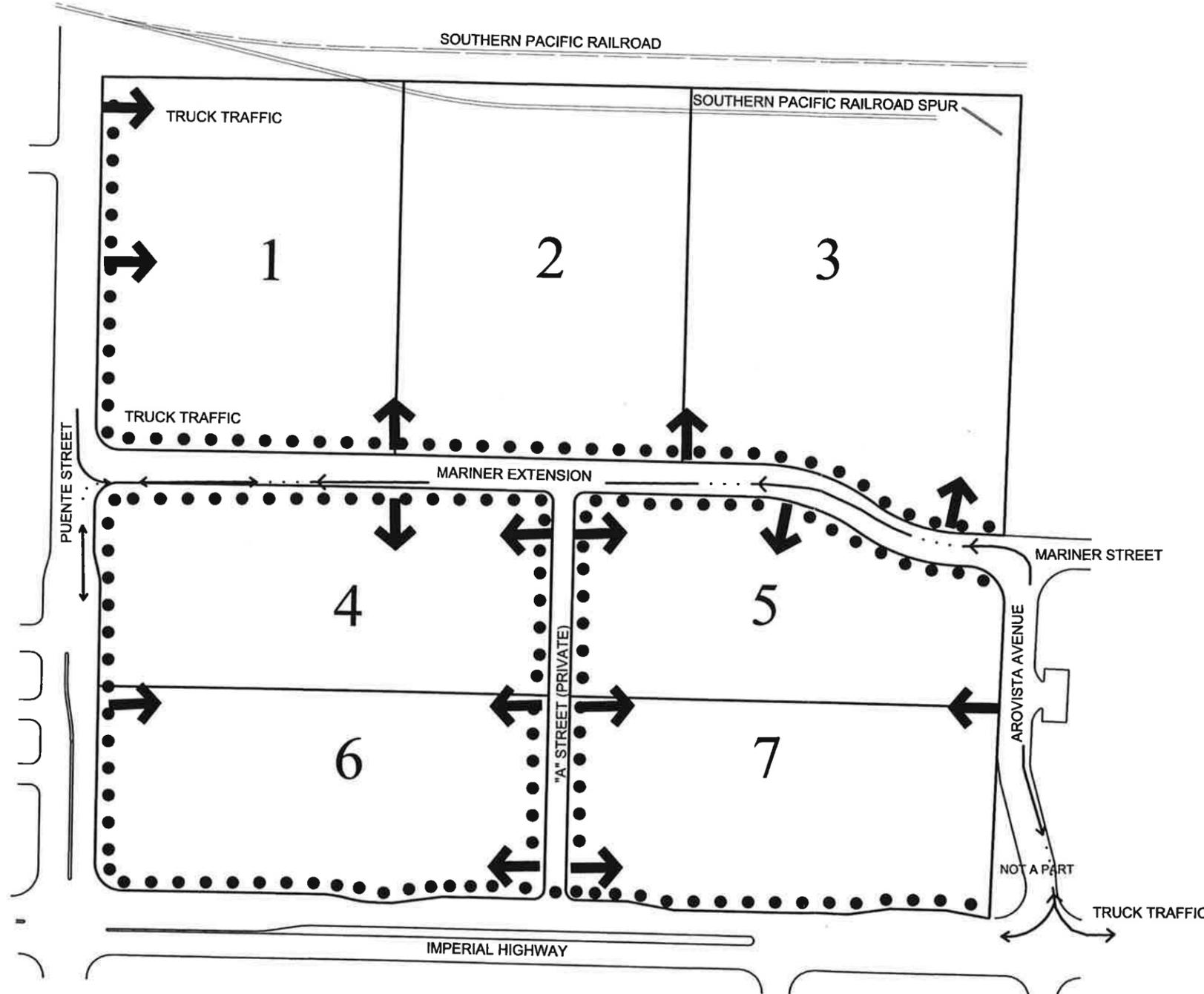
3.4.2 Pedestrian Circulation

The Brea Industrial Specific Plan provides exterior and internal circulation accommodations for pedestrians. The right-of-way for Imperial Highway will include a meandering sidewalk fronting the project site between Puente Street and Arovista Avenue. The meandering sidewalk will wrap around the corner and continue along the east side of Puente Street between Imperial Highway and the northern project boundary. In addition to the pedestrian sidewalks along Imperial Highway and Puente Street, pedestrian access to the project site will be available via "A" Street from Imperial Highway, and internally along both sides of the Mariner Street Extension. This will facilitate pedestrian access to both the northern and southern portions of the site. On-street parking shall be prohibited on Imperial Highway, Puente Street, "A" Street, and the Mariner Street Extension.

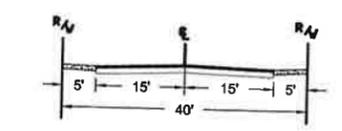
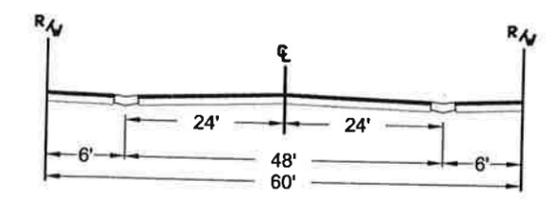
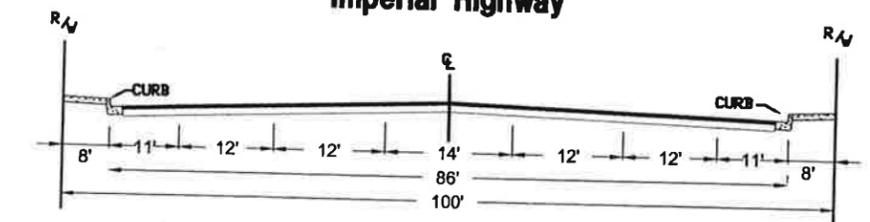
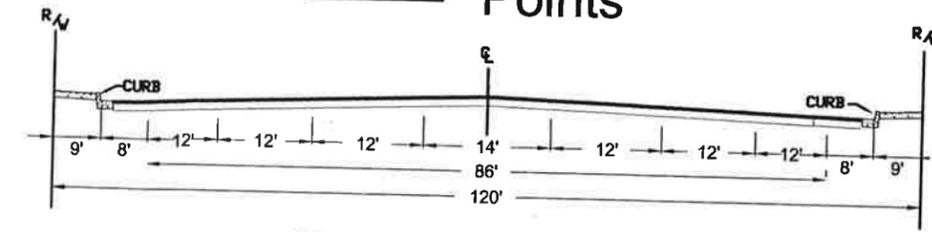
3.4.3 Rail Spur Access

A Southern Pacific Railroad (SPRR) spur currently exists on the project site immediately south of the northern project boundary, adjacent to the existing SPRR line. The reconstruction and use of the spur is optional and shall be implemented at the request of future users in Planning Areas 2 and 3. The spur shall enter the project near the northeastern corner of Planning Area 1 and be reconstructed to continue east along the northern portions of Planning Areas 2 and 3. Until the Spur is constructed, the area along the northern boundaries of Planning Areas 2 and 3 shall be utilized for parking. In the event that the railroad spur is constructed at a future date, this area of the project site shall be designed such that adequate parking is provided and conflicts between motor vehicle and rail traffic is minimized. In addition, traffic control devices as approved by the Southern Pacific Railroad, California Public Utilities Commission and City of Brea shall be constructed to further ensure safe interaction of motor vehicle and rail traffic. If a surplus of parking exists in this area, parking stalls may be removed.

CONCEPTUAL CIRCULATION PLAN



 Potential Access Points



**BREA INDUSTRIAL
SPECIFIC PLAN**

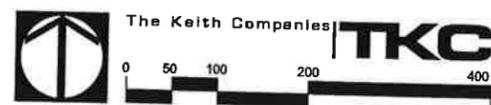


FIGURE 3-3

3.5 PUBLIC UTILITY PLAN

This section will describe the concept for the collection and disposal of project wastewater and for the distribution of water supplies to service each phase of development. The distribution of dry utilities, including gas, electrical and telephone services are also described. All utilities, wet and dry, shall be located underground. All off-site utility improvements shall be constructed per City of Brea standards and as approved by the City Engineer.

3.5.1 Sewer Plan

Sewer service for the seven (7) proposed Planning Areas will be provided from a variety of existing connection points. The Conceptual Sewer Plan for the Brea Industrial Specific Plan is depicted in Figure 3-4. Presently, there are two existing 8" Vitrified Clay Pipe (VCP) lines available to serve the site located in Imperial Highway along the southern project boundary and in Arovista Avenue along the southeastern project boundary. Planning Area 3 will be served by a 6" VCP line connecting to the existing 8" line located in Arovista Avenue and Mariner Street. The connection will occur at the existing manhole located directly east of the project boundary. Service for Planning Areas 5 and 7 will be provided by a 6" VCP connection to an existing stub from the 8" line in Imperial Highway at the southerly property boundary. Service to Planning Areas 2, 4 and 6 will be provided via a 6" VCP connection from a second existing stub on the 8" line located in Imperial Highway. Additional service to Planning Area 6 will be provided via a 6" VCP connection to a third existing stub from the 8" line in Imperial Highway. A 6" lateral connection to the existing manhole at the northeast corner of Puente Street and Imperial Highway will provide service to the eastern portion of Planning Area 4 and Planning Area 1.

3.5.2 Water Plan

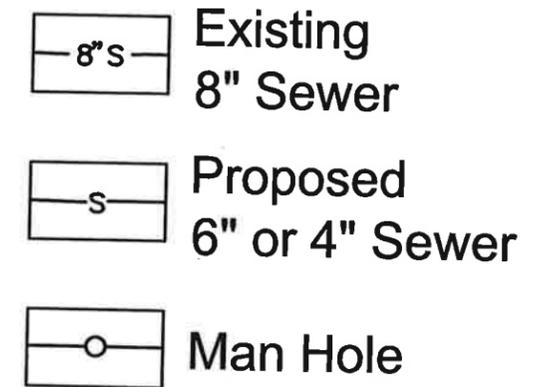
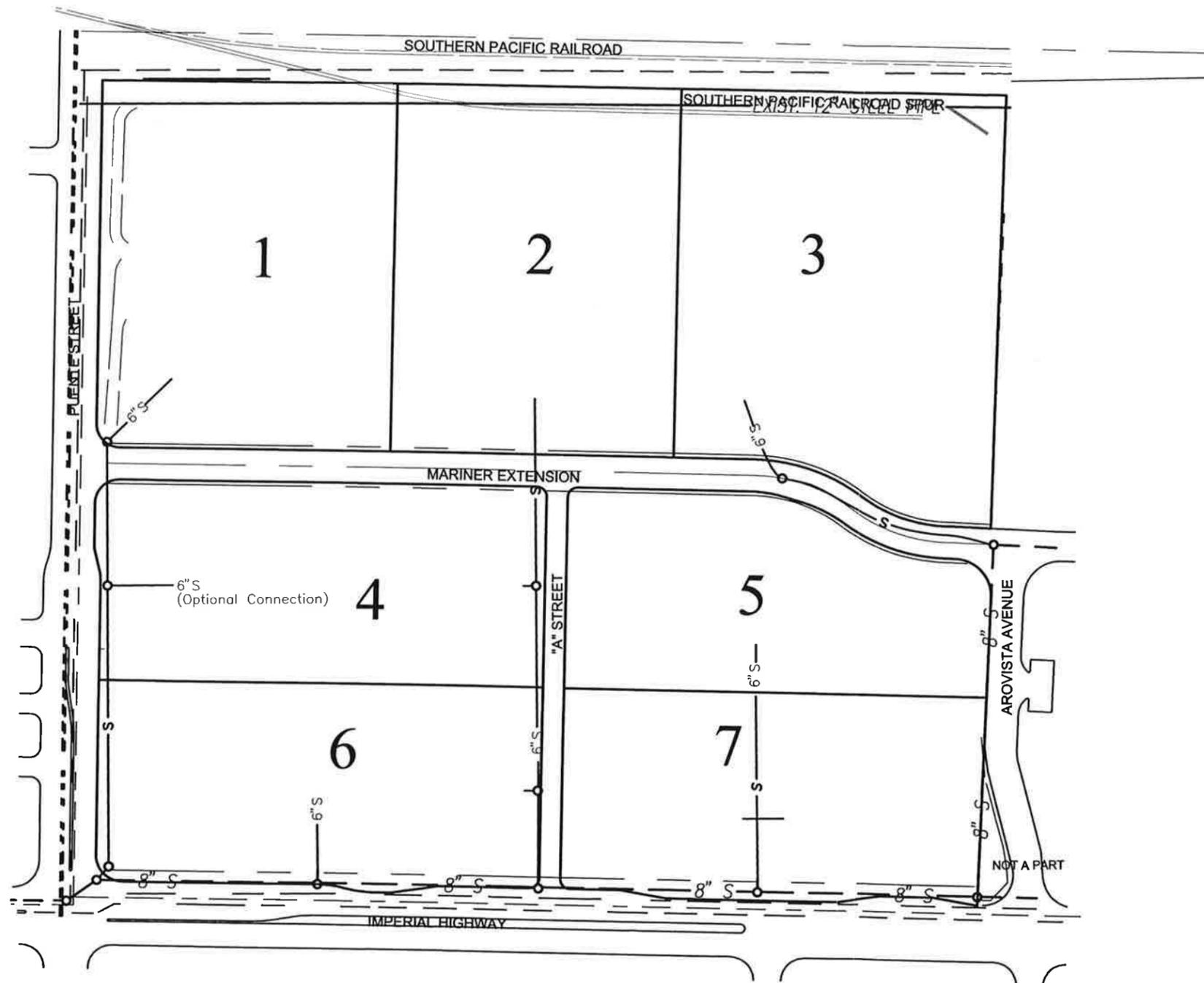
Water service is currently available to the project site from a number of locations. The Conceptual Water Plan for the Brea Industrial Specific Plan is depicted in Figure 3-5. One existing 12" Ductile Iron Pipe (DIP) line is located along the northern boundary of the project site, adjacent to the Southern Pacific Railroad line. A second 12" DIP line is located along the western boundary of the project site in Puente Street. A 6" DIP line and a 10" DIP line are located along the southern boundary of the project site in Imperial Highway.

Service to each of the Planning Areas will be provided by a proposed 12" DIP line in the Mariner Street Extension and in "A" Street. The proposed 12" DIP lines will be connected to an existing 10" DIP line located in Mariner Street at the eastern boundary of the project. The line along the Mariner Street Extension will be connected to the existing 12" DIP line located in Puente Street to complete the proposed loop. Individual 2" laterals will serve each of the Planning Areas through separate water meters.

3.5.3 Dry Utilities Plan

Southern California Gas Company and Southern California Edison Company shall satisfy the gas and electrical requirements of the project site, respectively. Pacific Bell shall provide telephone service and Century Cable shall provide cable television service. Presently, there are underground electrical, telephone, cable television, and gas facilities available to serve the site along Imperial Highway and Puente Street. Upon construction, all parcels will be provided with underground connections to electrical, telephone, cable television and gas services by the project developer. The City of Brea shall review and approve a dry utilities plan prior to commencement of construction. The installation of dry utilities shall be coordinated with other utilities including water and sewer.

CONCEPTUAL SEWER PLAN

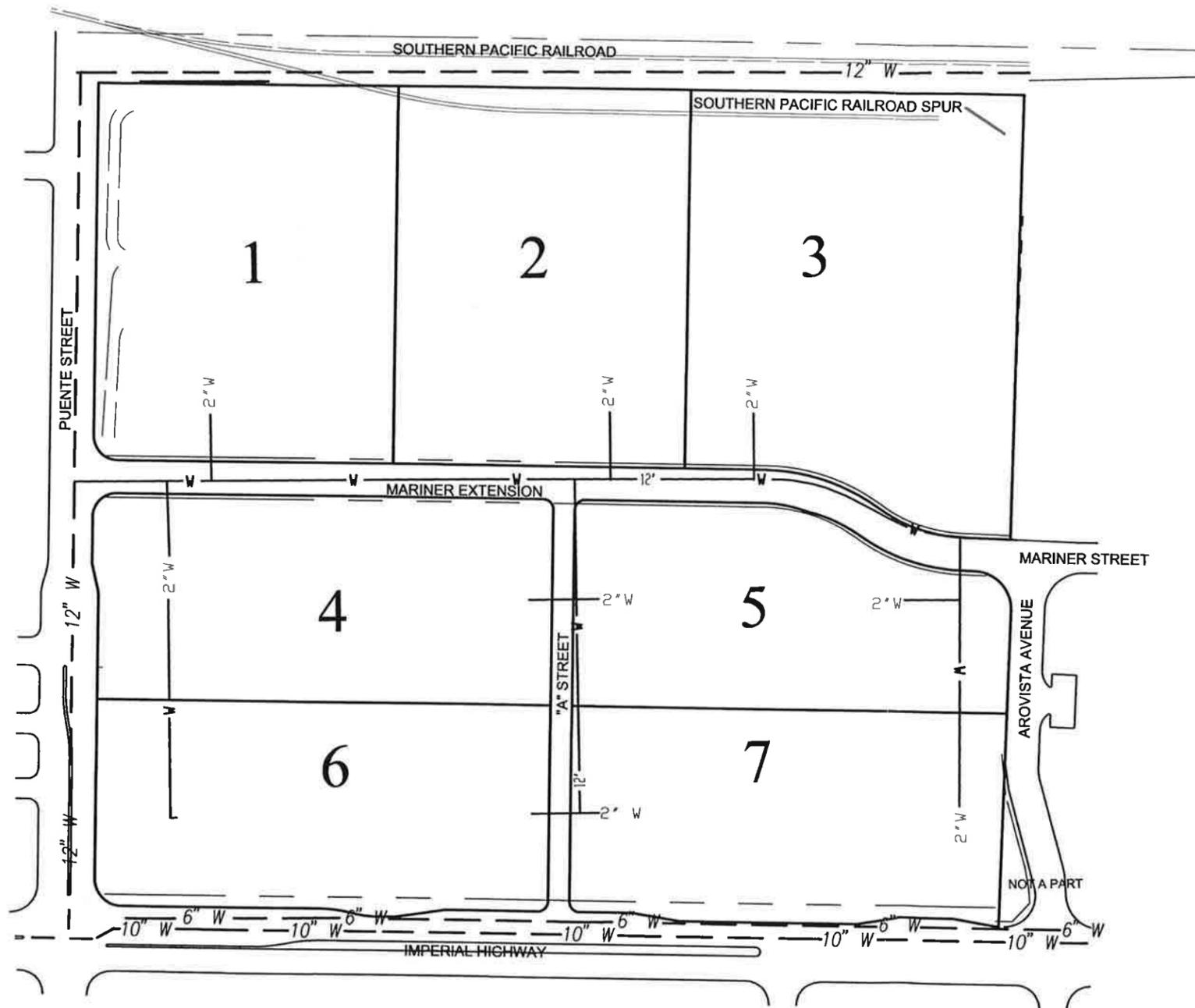


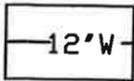
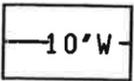
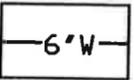
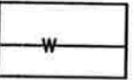
BREA INDUSTRIAL
SPECIFIC PLAN



FIGURE 3-4

CONCEPTUAL WATER PLAN



-  Existing Water
-  Existing Water
-  Existing Water
-  Proposed Water

BREA INDUSTRIAL
SPECIFIC PLAN

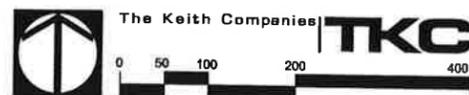


FIGURE 3-5

3.6 STORM DRAIN PLAN

This section describes the overall Storm Drain Plan that will be implemented during development of the Brea Industrial Specific Plan. The Conceptual Storm Drain Plan is shown on Figure 3-6. Final design shall be in accordance with City drainage standards and shall be approved by the Engineering Department of the City of Brea and the Orange County Flood Control District (OCFCD).

Storm drains are to be designed for a 25-year storm frequency for purposes of street flow and a 100-year frequency for protection to structures. The main storm drain lines within the public streets shall be dedicated to and maintained by the City of Brea. Pursuant to requirements of the State Water Resources Control Board, a National Pollution Discharge Elimination System (NPDES) construction permit shall be obtained for all construction activities including clearing, grading, or excavation that result in the disturbance of at least five acres of total land area or activity which is part of a larger common plan of development of five acres or greater. The developer shall obtain the appropriate NPDES permits prior to the commencement of grading activities. Best Management Practices shall be developed and employed during grading activities.

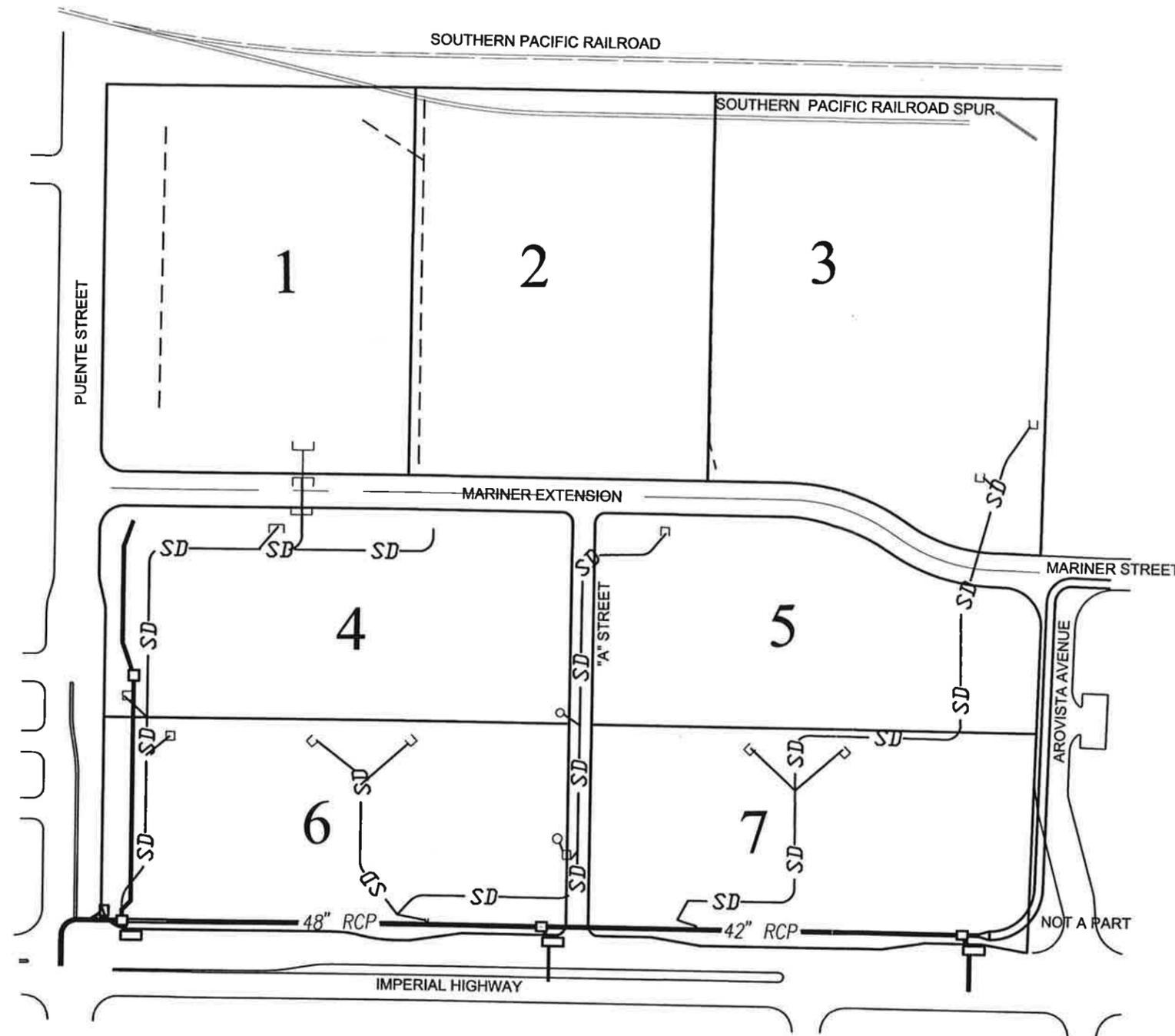
3.6.1 On-Site Flows

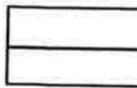
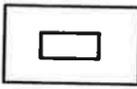
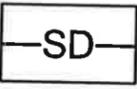
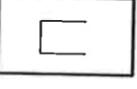
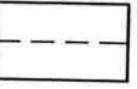
The proposed on-site flow direction is primarily from north to south. The drainage is intercepted at various proposed collection points (catch basins) located throughout the site. The on-site catch basins are designed and located to minimize parking area and street flooding and provide the most efficient storm drain collection system. Surface flows north of the Mariner Street Extension will flow southward via ribbon gutters to collection points as shown on Figure 3-6. All on-site storm runoff will be collected and conveyed to the existing 42" to 48" Reinforced Concrete Pipe (RCP) storm drain system located in Imperial Highway south of the Specific Plan area.

3.6.2 Off-Site Flows

An existing 42" to 48" RCP storm drain pipe is located in Imperial Highway. The existing drainage pattern on the Specific Plan area is such that this existing facility currently collects and will continue to collect drainage from the site as depicted in Figure 3-6.

CONCEPTUAL STORM DRAIN PLAN



-  Existing Storm Drains
-  Existing Detention Basin
-  Proposed Storm Drains
-  Proposed Detention Basin
-  Proposed Ribbon Gutters

**BREA INDUSTRIAL
SPECIFIC PLAN**

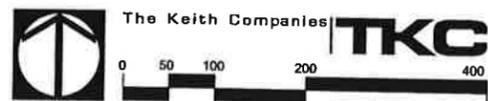


FIGURE 3-6

3.7 WALL AND FENCE PLAN

The purpose of the wall and fence plan is to assist in the establishment of project identity and compliment the architectural character of the development. The proposed perimeter walls are designed to alleviate noise from adjacent land uses. A buffer area of trees and shrubs will be planted in conjunction with walls to provide additional screening at sensitive areas. The wall and fence plan for the proposed project is depicted on Figure 3-7. A perimeter theme wall will be established on the north side of the project, south of the Southern Pacific Railroad right-of-way and along the eastern project boundary north of the Mariner Street Extension.

Project perimeter walls will be slump stone or split fence block with graffiti resistant finish and plant-on pilasters at 50 feet on center maximum. The project perimeter walls (theme walls) shall not exceed seven (7) feet in height. The following guidelines apply:

- A. All fencing and walls are to be provided by the builder of each Planning Area at the time of development and as approved by the City at time of submittal.
- B. All perimeter project walls are to be built during the first phase of development.
- C. To buffer adjacent land uses from each other, solid theme walls and/or landscaping are required for privacy and sound attenuation.
- D. All walls and fences used within the Brea Industrial Specific Plan area (e.g. storage areas) shall be of a material and color that is compatible with the architectural design of the structures (see Figure 3-8) subject to City approval.
- E. Chain-link fencing is permitted on access sides of storage areas, as long as the chain-link fencing is vinyl-covered with a fabric screen and cannot be viewed from public streets or the private "A" Street. Refer to Figure 3-8 for a conceptual rendering of the use of chain-link fencing in storage areas. Otherwise, chain-link fencing is prohibited. Metal gates for trash enclosures are required per City standards.
- F. Barbed wire is strictly prohibited.

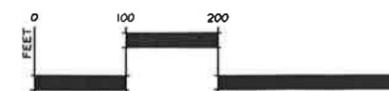
CONCEPTUAL WALL AND FENCE PLAN



 Project Theme
Wall

**BREA INDUSTRIAL
SPECIFIC PLAN**

FIGURE 3-7

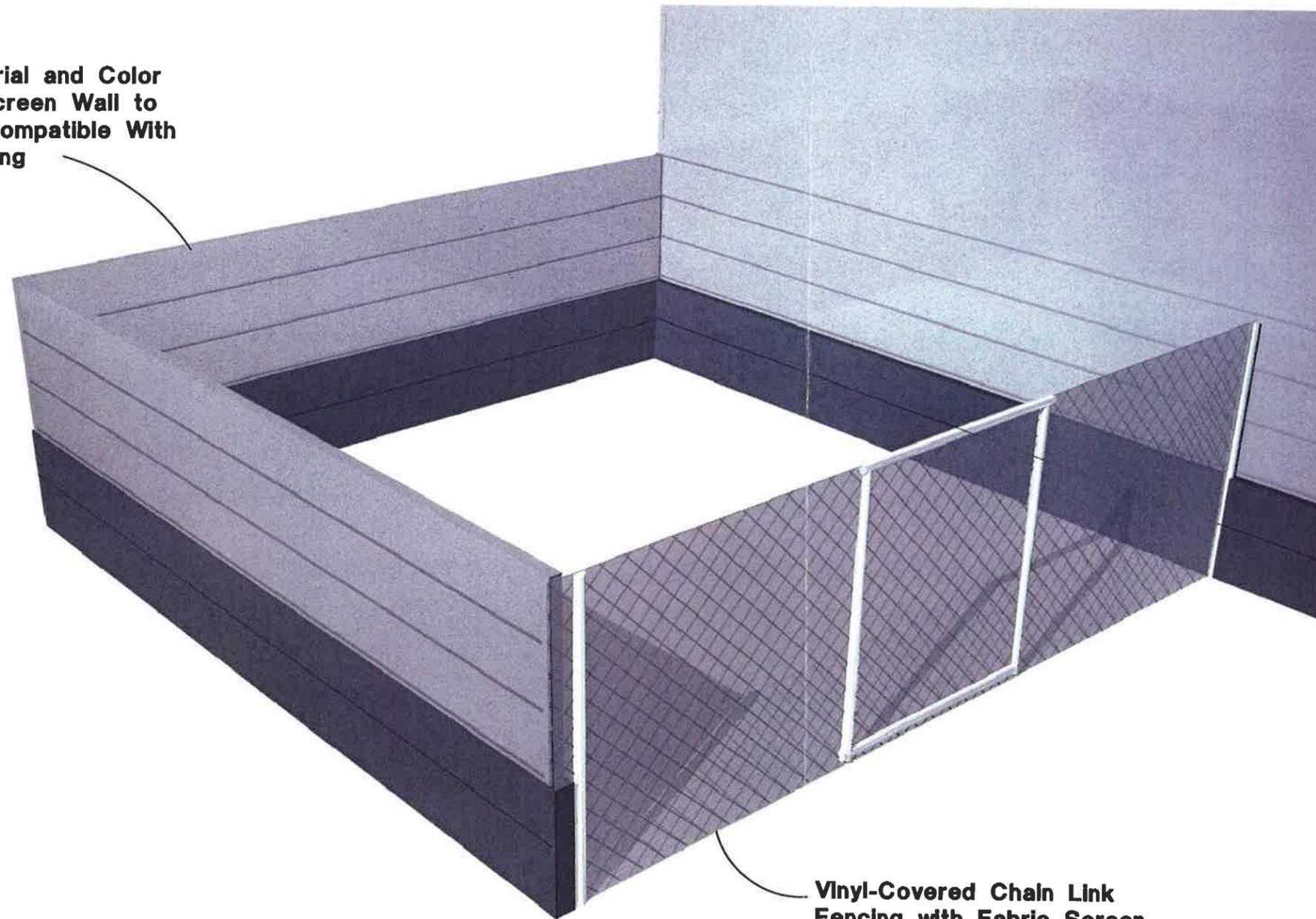


The Keith Companies
Inland Empire Division

N:\31384.000\DWG\SP1\FIG3-7.DWG JULY 1998

TYPICAL OUTDOOR STORAGE AREA

Material and Color of Screen Wall to Be Compatible With Building



Vinyl-Covered Chain Link Fencing with Fabric Screen

RENDERING - TYPICAL OUTDOOR STORAGE AREA

BREA INDUSTRIAL
SPECIFIC PLAN



FIGURE 3-8

4.0 DEVELOPMENT REGULATIONS AND PROCEDURES

SECTION 4.0 DEVELOPMENT REGULATIONS AND PROCEDURES

4.1 PURPOSE AND INTENT

The Brea Industrial Specific Plan is designed to promote a quality development through comprehensive designs and specific attention to quality details. The purpose and intent of these development regulations is to provide development criteria to guide developers and the City of Brea in the design of the proposed Specific Plan uses.

These regulations will serve as the primary mechanism for implementation of these land uses. The Regulations contained herein provide an appropriate amount of flexibility to anticipate future needs and to achieve compatibility between land uses. Principal land use designations for the Specific Plan shall be Light Industrial and General Industrial.

4.1.1 Light Industrial Zone (Planning Areas 4, 5, 6 and 7)

The purpose of the Light Industrial Zone is to provide appropriately located areas for the establishment of industrial plants and related activities, and to promote the concentration of such uses in a manner which will foster mutually beneficial relationships with each other, as well as with those areas of the city zoned for other development. The regulation of uses and standards of development set forth in the Light Industrial Zone are those deemed necessary to provide the proper environment for the efficient and desirable use of this type of industrial land, and to provide the proper safeguards to protect nearby residential, commercial, and public uses.

4.1.2 General Industrial Zone (Planning Areas 1, 2 and 3)

The purpose of the General Industrial Zone is to preserve the lands of the city appropriate for heavy industrial uses, to protect these lands from intrusion by dwellings and inharmonious commercial uses, to promote uniform and orderly industrial development, to protect property values, to foster an efficient and aesthetically pleasant industrial zone, to attract and encourage the location of desirable industrial plants, to provide an industrial environment which will be conducive to good employee relations and pride on the part of all citizens of the community, and to provide proper safeguards and appropriate transition for surrounding land uses.

SECTION 4.0 DEVELOPMENT REGULATIONS AND PROCEDURES

4.2 GENERAL REGULATIONS

4.2.1 Definitions of Terms

The meaning and construction of words, phrases, titles, and terms used in this Specific Plan shall be the same as provided in Brea Zoning Ordinance Section 20.00.070 Definitions, except as otherwise specifically provided herein (see Specific Plan Section 7.3, Definitions).

4.2.2 General Plan Consistency

The Brea Industrial Specific Plan is consistent with all elements of the City of Brea General Plan, as amended. Please refer to the Appendix for a complete discussion of General Plan consistency.

4.2.3 Zoning Ordinance Consistency

This Specific Plan regulates all development within Brea Industrial Specific Plan area. In cases where sufficient direction for interpretation of these regulations is not explicit in this text, the Brea Zoning Ordinance shall provide direction, as determined by the Development Services Director. In the event there are differences between this Specific Plan and the Brea Zoning Ordinance, the provisions and standards set forth in the Specific Plan shall prevail.

4.2.4 Building and Construction Codes Consistency

All construction and development within the Specific Plan area shall comply with applicable provisions of the Uniform Building Code (UBC) and the various related Mechanical, Electrical and Plumbing Codes, the Grading and Excavation Code, and the Subdivision Code as currently adopted by the City of Brea. In case of a conflict between the specific provisions of the codes identified above and the provisions of these regulations, the provisions of those Codes shall prevail.

4.2.5 Council Declaration/Severability

If any portion of these regulations is, for any reason, declared by a court of competent jurisdiction to be invalid or ineffective in whole or in part, such decision shall not affect the validity of the remaining portions of this Specific Plan. The City Council hereby declares that they would have enacted these regulations and each portion of this Specific Plan irrespective of the fact that one or more portions may be declared invalid or ineffective.

SECTION 4.0 DEVELOPMENT REGULATIONS AND PROCEDURES

4.2.6 Maximum Lot Coverage

In conformance with Section 4.0 (Development Regulations and Procedures) of this document, Lot coverage by structures shall not exceed 38 percent in the Light Industrial Planning Areas or 43 percent in the General Industrial Planning Areas for the proposed development. Lot coverage shall not include landscaping areas, parking lot areas or other non-occupied structures such as trash enclosures or covered parking structures.

4.2.7 Planning Area Boundaries

Planning area boundaries and acreages contained in this Specific Plan are approximate and based upon current information and a generalized level of mapping. Final planning area boundaries shall be established by the site plan and/or tentative/final tract map approval. Revisions to planning area boundaries shall not increase maximum allowable lot coverage as designated on Table 3-1.

4.2.8 Grading

Grading plans for all projects in the Brea Industrial Specific Plan area shall be consistent with applicable City of Brea codes and ordinances, and the most recent version of the UBC (for seismic criteria) as adopted by the City of Brea.

Grading shall be permitted within development planning areas of the Specific Plan, outside of an area of immediate planning area development, for the grading of public and/or private roads, infrastructure, and other development-related improvements. Remedial grading for development shall also be permitted in development planning areas outside of an area of immediate development to address geotechnical factors or soils engineering requirements.

4.2.9 Public Utilities

Public utility buildings, structures, and facilities including, but not limited to, electrical, water, sewage, telephone and television, and their storage, distribution, treatment and/or production required to carry out development are permitted in all planning areas.

SECTION 4.0 DEVELOPMENT REGULATIONS AND PROCEDURES

4.3 LIGHT INDUSTRIAL ZONE (PLANNING AREAS 4, 5, 6 AND 7)

4.3.1 Uses Permitted

No buildings or structures or land shall be used and no building or structures shall be hereafter erected, structurally altered or enlarged, except for the following purposes:

A. Principal Permitted Uses

1. *Manufacturing* - Subject to the conditions of this zone, manufacturing, assembling, repairing, testing, processing, warehousing, wholesaling, research or treatment of products may be conducted, other than those which may be obnoxious or offensive by reason of emission of odor, dust, smoke, gas, noise, vibration or other similar causes detrimental to the public health, safety or general welfare, including but not limited to the following:

- Aircraft and aircraft accessories
- Aluminum products
- Bakery wholesale
- Boat building
- Bottling plant
- Building material and hardware sales
- Cabinet shop
- Carpet cleaning plants
- Catering services
- Cleaning and dyeing plants, general
- Cutlery, hardware, hand tool, and utensil manufacturing
- Electric or neon sign manufacturing
- Food products processing, manufacturing, canning, preserving, and freezing (excluding fish and meat products, sauerkraut, vinegar, yeast and the rendering or refining of fats and oils)
- Furniture manufacturing, upholstering and refinishing
- Garage, parking or parking structure
- Garment manufacturing
- Hospital or medical clinic, industrial
- Ice and cold storage plant
- Laundries
- Machine shop
- Metal plating
- Metal working, light fabricating or welding firm
- Motion picture studio, laboratories and film processing

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- Paint mixing, provided a boiling process is not employed (no above ground tank farm or surface storage is permitted except above surface thinner storage is limited to two hundred (200) gallons and is permitted)
- Paper, fabrication form
- Plastic, fabricating form
- Printing establishments
- Product service centers
- Radio broadcasting studio or station
- Rubber products if:
 - The rubber is not melted
 - An internal type mixer is used, and residue therefrom is collected in accordance with rules and regulations specified by the Orange County Air Pollution Control District
- Sheet metal shop
- Telephone exchange, office and equipment building
- Television broadcasting studio or station
- Textile manufacture
- Training center, industrial
- Warehouses and storage buildings
- Woodworking shop

2. Other Uses

The following accessory uses are permitted only where they are integrated with and clearly incidental to a primary permitted industrial use:

- Dwelling, single, where used exclusively by a caretaker or superintendent of a permitted industrial use and his or her family
- Employees' cafeterias or coffee shops
- Exhibition of products produced on the premises or available for wholesale distribution
- Offices

B. Uses Permitted Subject to Conditional Use Permit

The following uses are permitted subject to approval of a Conditional Use Permit Application in accordance with the provisions of § 20.408.030 of the City of Brea Zoning Ordinance:

1. Any principal use permitted in the General Industrial Zone as specified in Section 4.4.1.A and subject to all provisions therein.

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2. Any conditional use permitted in § 20.248.020.B.2 of the City of Brea Zoning Ordinance, and subject to all the provisions therein, with the exception of Car Washes, Golf Courses, Motels and Billboards.
3. Administrative and professional offices (any office in which chattels or goods, ware or merchandise are not sold or manufactured).
4. Health clubs, studios or spas.
5. Astrology, fortune-telling, etc.
6. Retail outlets or showrooms.

4.3.2 Uses Expressly Prohibited

The following uses are expressly prohibited in the Light Industrial Zone:

- A. Residential uses, except caretakers quarters.

4.3.3 Property Development Standards

The following property development standards shall apply to all land and buildings in the Light Industrial Zone:

- A. *Lot Area:*

Each lot shall have a minimum area of six thousand (6,000) square feet.

- B. *Lot Dimensions:*

1. Width - Each lot shall have a minimum width of sixty (60) feet, except where adjacent to a residential zone, school or park, the width shall be not less than one hundred sixty (160) feet.
2. Depth - Each lot shall have a minimum depth of one hundred (100) feet, except where adjacent to a residential zone, school or park, in which case the minimum depth shall be two hundred (200) feet.

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B. Building Height:

1. Maximum Height Limit - Buildings and structures erected in the Light Industrial Zone shall have a height not greater than sixty (60) feet. Heights above the maximum may be permitted by conditional use permit, subject to the provisions of § 20.408.030 of the City of Brea Zoning Ordinance.
2. Structures Permitted Above Height Limit - Roof structures for the housing of elevators, stairways, tanks, ventilating fans or similar equipment required to operate and maintain the buildings and fire or parapet walls, skylights, towers, steeples, smoke stacks, flagpoles, chimneys, wireless and television masts, water tanks, or similar structures may be erected above the maximum height permitted in each zone. No structure shall be allowed for purposes of providing additional floor space.

C. Yards:

1. Where the Light Industrial Zone fronts, sides, or rears on any arterial street, or a local street which is a boundary with a residential zone, there shall be a yard abutting said arterial street or local street of not less than fifty (50) feet. The twenty (20) feet nearest the street shall be landscaped and maintained. The remainder may be used for parking.
2. Where the Light Industrial Zone fronts, sides or rears on a local street which is not a boundary with a residential zone, there shall be a yard of not less than ten (10) feet abutting said street.
3. In all front yards, the equivalent of one (1) tree per thirty (30) lineal feet of interior property line shall be provided in all rear and side yards, visible from adjacent streets or residential neighborhoods, one (1) tree for each thirty (30) lineal feet of combined rear and side interior property lines shall be planted in either a lineal or grouped manner. In addition, a five (5) foot, net (clear of curb), interior property line landscaped strip shall be provided. This landscaping shall be continuous along all interior property lines. Landscaping shall be held back from the property line or intersection with driveways or streets so as not to hinder traffic visibility.
4. All yards between the public street curbing and the property line are to be landscaped and maintained with active ground cover, such as green grass, dichondra, ivy, shrubs and/or trees.
5. All unpaved or undeveloped areas of a site for which a development application has not been submitted, shall be planted with a ground cover and/or shrub material

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as a condition of project approval. Undeveloped areas which are proposed for future expansion shall be kept in a weed free condition.

D. Walls, Fences, Landscaping and Property Maintenance:

1. Required Walls

- a. Walls along common property lines shall be erected as required by the City of Brea Zoning Ordinance.
- b. Required walls shall be constructed of masonry material not less than six (6) inches in thickness.

2. Permitted Fences and Walls

Fences and walls not to exceed seven (7) feet in height shall be permitted within any side or rear yard area or along any common property line provided, however, that the wall or fence does not exceed a height of thirty (30) inches within the front yard or side yard on the street side of a corner lot.

3. Height of Walls and Fences

The height of walls and fences shall be measured from highest ground level immediately adjacent to the base of the wall.

4. The provisions of this section shall not apply to a fence or wall height as required by any law or regulation of the state or agency thereof.

5. Landscaping Required

- a. Areas utilized for parking or loading, will be screened, modulated, or interrupted from view of access on adjacent streets, freeways, and adjacent residential property. This can be accomplished by one of three (3) techniques:
 - Lineal masses of shrubs;
 - Lineal or group masses of major scale trees; or
 - Lineal or grouped masses of smaller scale trees.
- b. Dual texturing of building facades or a five (5) foot strip of building perimeter landscaping shall be required, with the exception of rear or side walls abutting other existing buildings.

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- c. Parking Lot Trees. Trees equal in number to one (1) per each five (5) parking stalls either grouped or clustered shall be installed in all parking areas. Said trees shall be placed on the lot so as not to interfere with interior industrial parking lot circulation. Trees shall be placed so as to give relief to the monotony of rows of parked vehicles.
- d. A detailed landscaped plan (including irrigation, plant and material specifications) shall be submitted to the Development Services Department for its approval prior to the issuance of building permits.

6. Landscaping Maintenance

- a. Prior to the installation of the landscaping in public right-of-way, the developer shall provide for continued maintenance by an agreement with the City of Brea.
- b. Property owners or the appropriate maintenance district shall be responsible for the continual maintenance of all landscape areas on-site, as well as contiguous planted areas within the public right-of-way. All landscaped areas shall be kept free from weeds and debris, maintained in a healthy, growing condition and shall receive regular pruning, fertilizing, mowing and trimming. Any diseased, dead, damaged or decaying plant materials shall be removed and replaced within thirty (30) days following written notice from the Development Services Director.
- c. Irrigation systems shall be kept in working condition. Adjustments, replacements, repairs, and cleaning shall be a part of regular maintenance.
- d. Trees shall be staked and tied with lodge poles. Stakes and ties on trees shall be checked regularly for correct functions. Ties shall be adjusted to avoid creating abrasions or girdling on trunks or branches.
- e. Street trees planted within public right-of-way and/or trees planted adjacent to public sidewalk shall be provided with root barriers subject to Development Services Director review and approval.

7. Property Maintenance

Property owners or the appropriate maintenance district shall maintain all structures, including buildings, paved areas, accessory buildings and signs in the manner required to protect the health and safety of users, occupants, and the

SECTION 4.0 DEVELOPMENT REGULATIONS AND PROCEDURES

general public. The property shall be deemed substandard when it displays evidences of a substantial number of dilapidated conditions including, but not limited to the following:

- a. Faulty, sagging or leaking roof;
- b. Improper weatherization of building siding materials such as chipped or worn paint, wood siding showing signs of dry rot, cracked or chipped stucco and dented or rusted metal siding;
- c. Broken or missing windows and sign panels;
- d. Inadequate site drainage or standing water adjacent to building foundations;
- e. Broken or inoperable sanitary and plumbing facilities;
- f. Structural deficiencies;

E. *Distance Between Buildings:*

No requirements.

F. *Coverage by Structures:*

Lot area coverage by buildings or structures shall not exceed thirty-eight percent (38%) of the total lot area.

G. *Off-Street Parking and Loading:*

The provisions of § 20.08.040 of the City of Brea Zoning Ordinance shall apply.

H. *Access:*

All lots shall have a minimum of fifty (50) feet of access on a dedicated and improved street.

I. *Signs:*

1. Permitted Signs

- a. Temporary signage per City standards

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- b. A building permit and sign approval by the City of Brea shall be required for all tenant signs prior to installation.
- c. Except as provided herein, the provisions of Chapter 20.28 of the City of Brea Zoning Ordinance shall apply.

J. Colors:

Colors, materials, and finishes are to be coordinated on all exterior elevations of the buildings and walls to achieve total continuity of design. A color and materials palette shall be submitted for review prior to permit issuance.

K. Lighting:

- 1. Parking lot lighting fixtures are to have an overall maximum height that is consistent with the height of the buildings themselves. Walkway lighting fixtures are to have an overall maximum height of fourteen (14) feet.
- 2. Cut-off exterior light fixtures and their locations shall be submitted on a photometric plan for review by the City of Brea.
- 3. Security lighting fixtures are not to project above the fascia or parapet of the building and are to be shielded or recessed in the building walls to provide cut-off at the property line. No floodlights or wall-packs shall be permitted.

L. Outdoor Storage, Uses, and Waste Disposal:

1. Outdoor Storage

- a. All storage areas, loading areas and refuse collection areas shall be screened from public view by a concrete block or masonry wall or in such a manner so that it is not visible from adjacent property and abutting public right-of-ways.
- b. All noise producing equipment shall be so housed and located in such a manner that sound does not create a nuisance beyond any common property line abutting a residential zone and shall be screened in such a manner so that it is not visible from adjacent property and abutting public right-of-ways.
- c. Outdoor storage shall include all company owned and operated motor vehicles, except for passenger vehicles.

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- d. Storage or refuse collection shall not be permitted within front or side setback areas. Storage or refuse collection shall be permitted within the rear setback area.
- e. Raw materials stored within one hundred (100) feet of a dedicated street or residential zone shall not be stacked higher than six (6) feet.

2. Storage of Combustible Materials

- a. The storage of combustible materials shall be not less than twenty (20) feet from any interior lot line, and a roadway shall be provided, graded, surfaced, and maintained from the street to the rear of the property to permit free access of fire trucks at any time.

3. Waste Disposal

- a. No materials or waste shall be deposited upon a subject lot in such form or manner that they may be transferred off the lot by natural causes or forces. All waste materials shall be stored in an enclosed area and shall be accessible to service vehicles.
- b. Waste which might cause fumes or dust or which constitutes a fire hazard or which may be edible by or otherwise be attractive to rodents or insects shall be stored only in closed containers in required enclosures.

4. Exterior Enclosures

- a. All exterior electrical cage enclosures and storage tanks are to be screened from view from access or adjacent streets and residential neighborhoods, by a fence, wall or mature landscape materials.

5. Trash Storage Method and Location

- a. Trash enclosure location shall be subject to the approval of the Development Services Department.
- b. Trash enclosure shall be of masonry construction or approved alternate material. Structural design and gates shall be in accordance with city standards.

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M. Mechanical Equipment, Duct Work and Drains:

1. All roof mounted mechanical equipment and/or duct work, which is not screened by the roof parapet and is visible from an adjoining street, is to be screened by an enclosure which is detailed consistently with the building.
2. No mechanical equipment except for emergency equipment is to be exposed on the wall surface of a building. Electrical equipment shall be located within an electric room. No electrical equipment shall be exposed on a building face.
3. Plans for cyclone blowers, bag houses, tanks, dust collectors etc., shall be reviewed at the time of preliminary plan check to determine design integration with buildings and adjacent area. Screen walls shall be required for such structures which are visible from the public right-of-way or "A" Street.
4. Incinerator vents are to be located on the rear or "hidden" side of the building whenever possible.
5. No gutters or downspouts shall be exposed on the wall surface of a building.

4.3.4 Performance Standards

All uses within the Light Industrial Zone shall comply with provisions of Chapter 20.20 of the City of Brea Zoning Ordinance (Performance Standards).

SECTION 4.0 DEVELOPMENT REGULATIONS AND PROCEDURES

4.4 GENERAL INDUSTRIAL ZONE (PLANNING AREAS 1, 2 AND 3)

4.4.1 Uses Permitted

No buildings or structures or land shall be used and no building or structures shall be hereafter erected, structurally altered or enlarged, except for the following purposes:

A. Principal Permitted Uses

1. Uses permitted in Light Industrial Zone, Section 4.3.1.A
2. *Manufacturing* - Subject to the conditions of this zone, manufacturing, assembling, repairing, testing, processing, warehousing of products in an enclosed building may be conducted, other than those which may be obnoxious or offensive by reason of emission of odor, dust, smoke, gas, noise, vibration or other similar causes detrimental to the public health, safety or general welfare, including but not limited to the following:

- Automobiles, trucks and trailers
- Burial vaults and caskets
- Candles
- Canvas
- Carpets and rugs
- Cement products
- Chalk
- Clay pipe and clay products
- Composition wallboard
- Glass, but excluding blast furnaces
- Glazed tile
- Graphite and graphite products
- Hemp products
- Ink
- Jute products
- Metal foil
- Missiles and missile components, excluding explosive fuels
- Motors and generators
- Paraffin products
- Plastics
- Porcelain products
- Pulp goods
- Pumice

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- Putty
- Railroad equipment
- Sand and lime products
- Sisal products
- Starch and dextrine
- Steel products
- Stone products
- Structural steel products
- Tile
- Wire and wire products

2. Machinery manufacture, including electrical, air conditioning equipment, dishwashers, dryers, furnaces, heaters, stoves, and washing machines.
3. Machine tool manufacture, including metal lathes, presses and stamping machines, and woodworking machines.
4. Manufacturing of chemicals and chemical products, except those requiring a conditional use permit.
5. Transit and transportation terminals, repair and storage facilities.
6. Lumber processing and woodworking, including planing mills, plywood, veneering, wood preserving, and laminating.
7. Electrical distribution substations, electrical transmission substations, gas metering and regulation stations, public utility buildings and service and storage yards, and steam electric generating stations including microwave facilities incorporated as part of a public utility installation.

B. Uses Permitted Subject to Conditional Use Permit

The following uses are permitted subject to approval of a Conditional Use Permit Application in accordance with the provisions of § 20.408.030 of the City of Brea Zoning Ordinance:

Manufacturing of:

- Acetylene
- Ammonia
- Aniline dyes
- Asphalt or asphalt products

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- Bleaching powder
- Bronze, babbitt metal and similar alloys
- Carbide, Carboric, hydrochloric, picric and sulfuric acid
- Caustic soda
- Cellulose and celluloid
- Cellophane
- Cement, lime, gypsum or plaster of paris
- Charcoal, lampblack or fuel briquettes
- Chlorine
- Creosote
- Exterminating agents
- Fertilizer
- Film
- Gas
- Lacquer, shellac, turpentine, varnish or calcimine (Kalsomine)
- Linoleum or oil cloth
- Matches
- Nitrating of cotton and other materials
- Phenol
- Potash
- Phroxylin
- Rubber (natural or synthetic)
- Soap, tallow, grease and lard

4.4.2 Uses Expressly Prohibited

The following uses are expressly prohibited in the General Industrial Zone:

1. Residential uses, except caretakers quarters.

4.4.3 Property Development Standards

The property development standards of the Light Industrial Zone shall apply to all property in the General Industrial Zone except:

1. Where a General Industrial Zone abuts any other industrial or commercial zoning district classification, a yard area of fifty (50) feet shall be maintained along said abutting district. This yard may be used for parking.
2. Lot coverage by buildings or structures shall not exceed forty-three percent (43%) of the total lot area.

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4.4.4 Performance Standards

All uses in the General Industrial Zone shall comply with the provisions of Chapter 20.20 of the City of Brea Zoning Ordinance (Performance Standards).

5.0 DESIGN GUIDELINES

5.1 PURPOSE AND INTENT**5.1.1 Purpose**

The Brea Industrial Specific Plan Design Guidelines will be utilized to direct the future physical development of the Specific Plan area. The Design Guidelines are intended to provide a framework for architectural and landscape design practices. The purpose of the Design Guidelines is assist developers, builders and architects in creating an aesthetically pleasing, cohesive and unified industrial, office and warehousing/distribution project for users within the City of Brea. The Design Guidelines will also assist City of Brea staff, Planning Commission, City Council and other City decision-making entities when reviewing development projects within the Specific Plan area.

It is the intent of the Design Guidelines to provide direction on a project-wide level as well as a planning area and site specific level. In the event of consolidation of parcels for a large tenant, these Design Guidelines shall provide flexibility, but maintain the goals as set forth below. In addition to meeting the criteria of the Design Guidelines, all improvements must conform to the appropriate county, state and federal building requirements.

5.1.2 Goals

The Design Guidelines include architectural, landscape, signage and lighting standards which are intended to encourage commercial and industrial site development that fulfills the following goals:

- Provides an overall cohesive and compatible environment for business and industrial uses.
- Maintains compatibility with existing and proposed surrounding land uses by buffering uses adequately.
- Encourages a consistent and compatible design theme through the use of similar building design elements.
- Provides visually pleasing views from all off-site locations through well articulated building facades, landscape enhancement and a “cohesive” signage and lighting program.

5.1.3 Art in Public Places

Public sculpture gives identity and dimension to a new project. The Brea Industrial Specific Plan area shall participate in the City's Art in Public Places Program with the contribution of sculptures to Brea's outdoor gallery. Figure 5.3.1 illustrates three proposed locations for art pieces including one at the intersection of Puente Street and the Mariner Street extension, one at the intersection of Puente Street and Imperial Highway and one at the project main entrance from Imperial Highway. The concept landscape design details shall accommodate and enhance the placement of the proposed art pieces. In addition, there is an existing art piece located at the intersection of Imperial Highway and Arovista Avenue, which is not a part of the Specific Plan area, but shall remain in its existing location. The actual contribution and locations of the proposed art pieces are subject to the approval by Brea's Development Services Department and Art in Public Places Committee. Landscaping and lighting shall be designed to maximize viewing and enhancement of the artwork.

5.2 ARCHITECTURAL GUIDELINES

The purpose of establishing Architectural Design Guidelines is to establish the development criteria at the project level to assure a unified environment with a master planned design. The intent is not to require rigid adherence to a particular architectural style on each and every structure or to limit the range of materials or colors used throughout. More correctly, these guidelines should be used to achieve continuity and establish a larger visual context.

The selection of an architectural style is meant to fulfill two specific goals. The first goal is to establish a sense of place within the City of Brea. This can be accomplished by utilizing a singular and recognizable architectural style within which all structures, signage and lighting will be designed. Each element of the project will thus have an identifiable relationship with the total designed environment. The second goal is to respond to the orientation and existing nature of the area. Attractive, durable, high quality materials with sensible attention to maintenance will be used.

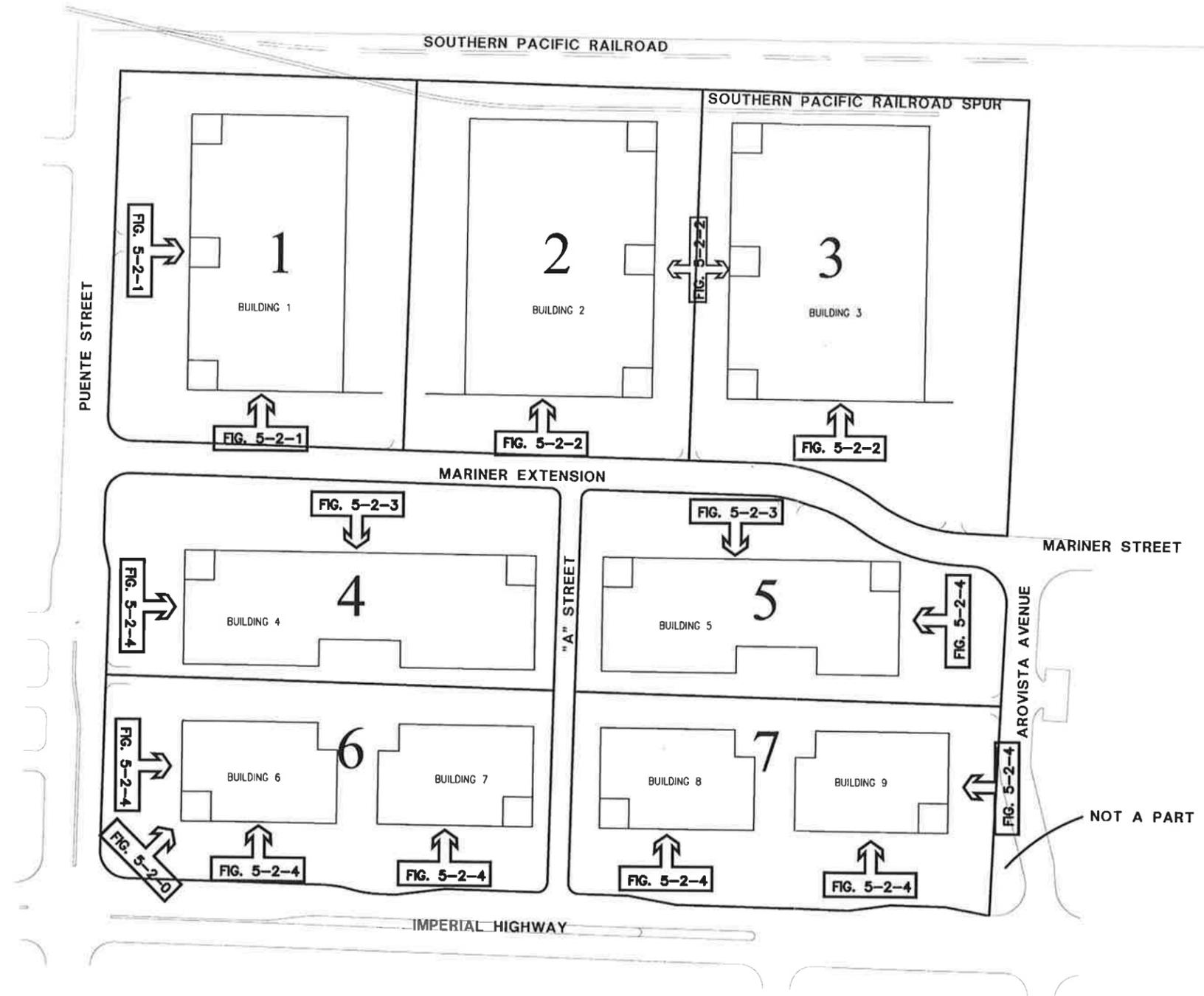
When reviewing the following section, refer to Figures 5-2-1 through 5-2-4 which depict architectural renderings of a conceptual entry way and building elevations for the proposed uses. Figure 5-1 provides a key illustrating the location of the architectural renderings throughout the project.

5.2.1 Building Facades

The following information will assist with evaluation of architectural elements of building facades proposed within the Specific Plan area:

- Pedestrian and ground level **building entries** shall be recessed or covered by architectural projections, roofs and arcades to provide architectural and visual relief. Insets, columns, projections and exterior material changes shall be used to architecturally demarcate and create interesting building entries.
- Landscaped areas with bench seating and other aesthetic elements can be used as a **focal point**, or center of confluence for several buildings grouped together.
- **Exterior openings** of buildings such as entries should be inset to provide visual relief and interest, which identifies separate tenant entry areas. Other doors and windows should be used in a repetitive pattern to create rhythm, but care should be taken to avoid monotony.
- Long and straight **building facades** which face public views shall be architecturally articulated by varying building mass, forms, textures, or colors. Landscaping shall also be used to create interest and soften building facades.
- Ground level stories of buildings adjacent to walkways and roadways should be architecturally articulated (introduction of smaller, pedestrian scale features such as lower building masses, signage, doors, light fixtures, planters, etc.) to create **human scale**.

BUILDING ELEVATION LOCATION PLAN FIGURES 5-2-1 TO 5-2-4



- 3 PLANNING AREAS
- ↑ BUILDING ELEVATION
FIGURE LOCATIONS

**BREA INDUSTRIAL
SPECIFIC PLAN**

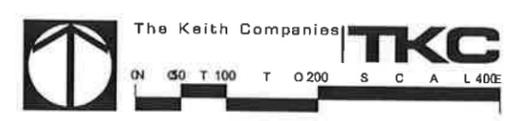


FIGURE 5-1

CONCEPTUAL
PERSPECTIVE



RENDERING - CORNER OF IMPERIAL HIGHWAY & PUENTE STREET

BREA INDUSTRIAL
SPECIFIC PLAN



FIGURE 5-2-0

N:\31384.000\DWG\FIG 5-2-0.DWG JANUARY 2000

BUILDING 1 CONCEPTUAL ELEVATION



RENDERING - MARINER EXTENSION - BUILDING 1



RENDERING - IMPERIAL PUENTE ELEVATION - BUILDING 1

BUILDING 2 & 3 CONCEPTUAL ELEVATION



RENDERING - MARINER EXTENSION - BUILDING 2 and 3



RENDERING - FRONT ELEVATION - BUILDING 2 and 3

BUILDING 4 & 5 CONCEPTUAL ELEVATION



RENDERING - TYPICAL MARINER EXTENSION ELEVATION - BUILDING 4, BUILDING 5 SIMILAR

BUILDING 4 & 9 CONCEPTUAL ELEVATION



RENDERING - PUENTE STREET ELEVATION - BUILDING 4 and 6
RENDERING - AROVISTA AVENUE ELEVATION - BUILDING 5 and 9



RENDERING - IMPERIAL HIGHWAY ELEVATION - BUILDING 6, 7, 8, and 9

- All roof top *mechanical equipment* shall be screened by the building parapet or otherwise kept from public view. Screening and shielding elements shall be integrated and compatible with building design, subject to City approval.
- *Roofs of buildings*, as viewed from other buildings, adjacent parcels and public streets shall appear clean, organized and uncluttered.
- *Service entries* shall be shielded from public view by being placed towards the rear and nonpublic view sides of buildings.
- *Service loading, utility and trash collection areas* shall be screened from public view (public streets, parking areas, pedestrian walks, etc.) by permanent walls and landscaping. Screening walls and landscaping should be integrated with building treatments. All outdoor storage and refuse collection areas shall comply with the Development Regulations and Procedures outlined in Section 4.0 of this Specific Plan.

5.2.2 Exterior Materials

The following exterior guidelines are to assist in formulating a cohesive style of architecture that is indicative of predominately concrete tilt-up construction types. Therefore, through the architectural design of their facilities, it is intended that these businesses express themselves individually.

- Individual buildings within Specific Plan area shall be designed with a limited number of *materials* to simplify exterior elevations and create a clean linear contemporary appearance.
- Accent building face areas or *accent bands* of contrasting materials, patterns, textures or colors shall be used to create interest, focus, unity, and compatibility.
- Traditional *architectural styles* such as Victorian, Gothic, Spanish Revival, English Tudor, etc. shall not be permitted, nor shall pre-fabricated mobile metal or metal-sided buildings of any kind be allowed.
- These buildings, as a group, will *not be residential* in character, thus residential quality and selection of materials will be strongly discouraged.
- *Building appendages* such as wing wall, canopies, roof projections, feature elements, etc. must be an integral part of the overall architectural design.
- Special consideration shall be given to the *visitor and employee entrances*, entry plazas and parking areas to ensure an aesthetically pleasing visual experience.

- If exterior *theme walls* are designed as part of the project, they shall be constructed of concrete or masonry materials. The design, color and finish shall be compatible with the architectural design of project buildings.
- *Specific design consideration* should be given to:
 - Street Orientation and Drives
 - Character of Surrounding Buildings
 - Vehicular, Railway and Pedestrian Traffic Patterns
 - Landscaped Areas

Exterior Wall Surfaces

- All *exposed surfaces* shall be treated with a finish material and shall not be left in an unfinished or temporary state. To ensure continuity among buildings constructed, generally acceptable exterior building materials are:
 - Pre-cast concrete with textured and/or painted finish
 - Poured-in-place concrete with similar treatment
 - Concrete masonry units, painted, stained or ceramic clad
 - Tile or tile panel system
 - Glass or glazing units
 - Glass blocks
 - Natural stone or stone veneer (as accent)
- No mechanical or electrical equipment shall be exposed on a *building face*. Electrical equipment, meters, etc. shall be located inside of buildings behind man-doors.

Exterior Wall Colors

- *Predominant wall colors* shall be in the white-white, off-white, or grey-white ranges. Up to five exterior colors as shown in Figures 5-2-0 through 5-2-4 may be provided. Additional colors of glass may be used to add variety to the overall building design. The use of primary colors or dark earth tones shall be discouraged unless limited to an accent band or highlight. Light pastels will only be permitted subject to special design review and approval by the property owner's architect. Consideration will be given in these submittal reviews for context and overall compatibility to neighboring buildings within the project.

Roofscapes

- **Roof of buildings** as viewed from neighboring buildings within the project and from off-site areas shall appear clean, organized, and uncluttered.
- **Mechanical equipment** shall be screened from view by the building parapet and be insulated accordingly for sound attenuation.
- **Rooftop mounted** satellite “dish” antennas are specifically prohibited, unless they are screened from public view. Generally, to the rear of the building is acceptable with final location subject to review and approval by the City’s Development Services Director, prior to installation.
- Under no conditions, shall **rooftop mounted** signs or lights be permitted.
- **Skylights** shall be designed as an integral part of the design of the particular building. The glazing type shall be clear translucent or heat tempered and/or wired glass with frames to match the roof material.
- Vents and all other **architectural projections** shall match the roof color and should otherwise be inconspicuous in nature. To the extent feasible, these items should be screened by the building design. A general grouping of necessary, but rather unsightly items such as grease ducts, cooling fans, solar panels and equipment, etc., shall be installed as required by code and shielded from view by parapet walls and screens.
- All **flashing and sheet metal** materials shall be painted and treated to match adjacent materials unless copper is used. Gutters and down spouts shall also be treated to match adjacent materials unless designed as a continuous architectural feature, such as a cornice cap. Otherwise, they are to be concealed within a building component such as a column or wall.

5.2.3 Landscape

To maintain a cohesive environment throughout the Specific Plan area, it is necessary for each builder to address the following landscape provisions in conjunction with the Landscape Guidelines discussed in Section 5.3 of this Specific Plan.

- **Project monumentation and landscaping** shall be installed by the developer whose property has been designated to contain a monument.
- **Landscape berms** installed adjacent to Imperial Highway and Puente Street shall be a unifying design element as viewed from offsite.

SECTION 4.0 DEVELOPMENT REGULATIONS AND PROCEDURES

4.5 SIGN REGULATIONS

Sign standards are required to establish a project-wide format and style and to regulate the appearance of graphics that are utilized for individual projects within the Brea Industrial Specific Plan area. The signs shall be architecturally attractive and contribute to the retention of a positive character within the site. These signs shall not compete with each other or dominate the setting. An effective signing and graphics system shall function as an integral part of the built environment. Carefully planned signs communicate essential information, while enhancing the architectural environment.

Signs within the Brea Industrial Specific Plan area shall be governed by a sign program as stipulated in Section 20.28.340.C.4 of the Brea Zoning Ordinance. The sign program shall be subject to approval of a Conditional Use Permit as stated in the provisions of § 20.408.030 in the Brea Zoning Ordinance. The primary purpose of the sign program is to provide design standards and details for visually coordinated and aesthetically attractive signage that is consistent with Section 5.2.4 (Signage Design Guidelines) of the Specific Plan. The following sign regulations shall be incorporated into the design of project signage:

- The **primary entry signage** shall provide project identity and visibility at the point of entry most frequently encountered by people. This point has been identified as the intersection of Imperial Highway and "A" Street.
- The **secondary entry signage** shall provide project identity and visibility at the Mariner Avenue extension intersections with Puente Street and Arovista Avenue.
- **Tertiary entry signage** shall provide project identity to individual planning areas. A similar scaled down version of secondary entry signage shall be utilized with minor modifications.
- The **street signs** will identify all streets within the specific plan area. They shall be located at intersecting right-of-ways. Location of street signage identifying such items as "Speed Limit" and "No Parking Area" shall be approved by the City Traffic Engineer.
- Project **marketing signs** will identify the General and Light Industrial projects from Imperial Highway and Puente Street with one sign per roadway allowed. All wood construction with a painted sign surface shall be utilized. The signs may be double faced.
- **Construction and leasing signs** shall be permitted to inform the viewer of new buildings and leasing opportunities, completion dates, and builder and leasing agent names and telephone numbers.

SECTION 4.0 DEVELOPMENT REGULATIONS AND PROCEDURES

- All other *project signage* shall conform to the Design Guidelines stated in Section 5.2.4 (Signage) and as outlined in Section 4.0 (Development Regulations and Procedures) of the Brea Industrial Specific Plan.

- **Landscape berms** should be continuous and undulating with meandering masses of trees and shrubs to minimize the impacts of buildings and visually screen parking areas.
- Plant material, special paving, planter walls, site furniture and pottery are **site design elements** which should be used to create interest, focus, and a design theme for the project.
- **Landscape design** shall be used to screen ground-mounted equipment, transformers, backflow preventors, etc.
- Special care shall be given to **selection of plant material** when designing and constructing landscaped areas. Landscaping materials should incorporate drought tolerant species when appropriate.
- **Screening and shading** of parking areas with interspersed planting areas should be considered when designing project landscaping.
- **Tree groupings** should be utilized to soften the scale and mass of buildings.
- **Planter tubs** of similar style can be used to add seasonal color in planting areas or hardscaping.
- **Tall planting** should be utilized to reduce scale and vertical mass of building.
- **Low level planting** with high color and varying texture adds visual interest at building perimeter and walkways.

5.2.4 Signage

The following is to provide guidelines necessary to achieve a visually coordinated, balanced and aesthetically appealing signage environment for the Brea Industrial Specific Plan.

- All signs shall meet or exceed the **minimum standards** set forth by the City of Brea Sign Ordinance. Any nonconforming signs shall be removed by the tenant at their expense, upon demand of the property owner or City official.
- The **tenant or property owner** shall pay for all signs, their installation (including final connection, transformers, and all other labor and materials) and maintenance.
- The **tenants or property owners** shall be responsible for fulfillment of all pertinent requirements or regulations and shall obtain all necessary permits required to install signage.

- The owners shall provide **primary electrical service** terminations at the interior of wall or canopy located in the center of the allowed signage area. It is the sole responsibility of the tenant's sign company or contractor to verify all conduit and transformer locations and service prior to fabrication and installation.
- **Address numbers** shall be logically applied in highly visible locations for view by vehicular traffic on streets adjacent to this project, as required by the City of Brea Fire Department.

No exposed raceway, crossovers, conduits, conductors, transformers, etc., shall be permitted.

- No more than two rows of **lettering or numbers** are permitted, per tenant, provided their maximum total height does not exceed the net sign area allowed by the Brea City Code.
- Tenants shall **display** only their established trade name of their basic product name, e.g., "John's Apparel", or individual combinations thereof.
- For purposes of **tenant identification**, each tenant will be permitted to place upon its main entry area doorway, not more than 144 square inches of painted or decal application lettering, not to exceed 2 inches in height, indicating hours of business, emergency telephone, etc. (The number and letter type shall be subject to approval by the property owner.) Company logos and business insignias shall be excluded from these standards.
- Individual **building monumentation** shall be limited to one (1) monument sign per parcel. These will be located a minimum of five (5) feet behind property right-of-way lines. For parcels with two (2) street frontages, a second monument sign shall be permitted.
- **Ground monument** and first story level signs can be used for buildings oriented to street traffic or located in a "campus-type" setting. Monument signs should be of a complementary design, color and material to the parent building(s).
- All **penetrations** into structures required for sign installation shall be sealed in watertight conditions and shall be patched and/or painted to match adjacent finish to the satisfaction of the property owner.
- A consistent size, location, color and graphic style of **signage** shall be used to create unity and rhythm on building facades.
- Signage can be used as an **accent feature** in building facades.

- **Temporary signs** shall be permitted for promotion of property development, sale or lease only. All temporary signs shall be located within property lines. No permanent lighting shall be allowed. All temporary signs require the review and approval of a Temporary Sign Permit Application and may require plan submission and construction permits through the City of Brea Building and Safety Department.

5.2.5 Lighting

- **Lighting** shall be located along streets, at building entries, in parking areas and loading areas, and shall be in character with building designs.
- Exterior lighting shall supply **adequate light levels** to ensure safety and security.
- **Pole mounted fixtures** for parking areas shall be designed to direct all light toward parking surfaces. Parking area luminaires shall be high pressure sodium vapor. Fixture design should be consistent throughout all planning areas.
- All lighting shall be positioned to eliminate **reflected or direct** light and glare onto adjacent properties.
- Lighting of the **parking areas** visible from the street shall be of a sharp cut off type preventing light spillage on to neighboring lots.
- Lighting of a **building exterior** is permitted in order to enhance architectural design elements.
- **Decorative lighting fixtures** such as wall sconces and bollard lights that are an integral part of the design of the building design are encouraged.
- **Security lighting** of rear and side yards is permitted if care is taken to prevent light spillage into neighboring properties or into the sky. However, no flood lights or "wall-packs" shall be permitted.
- **Outdoor lighting** is generally located along streets, in parking areas, at intersections and crosswalks and where various types of circulation intersect, split or change direction.
- **Entrances** to structures and pedestrian or vehicular intersections shall be emphasized with higher illumination levels, to ensure safety and a sense of security.
- **Minimum illumination levels** and recommended lighting practices have been established by the Illumination Engineering Society (I.E.S.).
- **Recessed incandescent** or HID lamp fixtures mounted in soffits over the doors and windows will be allowed.

- *Excessive use* of fluorescent type fixtures is discouraged and exposed light fixture cabinets of any type are prohibited.
- Lighting shall also be provided along pedestrian ways and plazas. Free-standing pedestrian scale lighting shall be consistent throughout all planning areas.

5.3 LANDSCAPE GUIDELINES

The purpose of the landscape guidelines is to provide design criteria for the landscape treatment throughout the Brea Industrial Specific Plan area. The landscape is a crucial element in establishing a total theme while at the same time providing a pleasing and unified environment for the project site. Landscaping enhances the visual quality and unity of the differing building types. By planning the landscape, overall consistency can be obtained and a cohesive unified link throughout the development achieved.

The landscape guidelines shall be used by the City and developers as a means of achieving the following project goals:

- Ensure that landscape considerations include low water consumption and erosion control.
- Define specific plant materials and groupings to achieve community identity.

To help achieve project distinction, a landscape plan has been prepared which reflects careful consideration of the need to conserve water. The use of grass has been limited to parkways where it is necessary to remain consistent with existing adjacent landscape. Decomposed granite, drought-tolerant shrubs and ground covers shall form the base of the landscape plan with significant planting of trees, both young and mature, to provide a canopy of greenery accented by flowering shrubbery.

The Conceptual Landscape Plan consists of a description of the landscape concept and illustrations of its implementation as it relates to street treatments, entry features and transition areas. A verbal description of the project-wide landscape plan coupled with summaries of the various plan components are detailed below.

5.3.1 Landscape Concept

The landscape theme is intended to support the architectural guidelines by defining spaces, creating moods and providing screens and buffers where needed, while interfacing the proposed project with adjacent land uses. It is vital that the landscape plan strives to emulate the design criteria set forth in these guidelines. The landscape concept is illustrated in the overall Conceptual Landscape Plan, Figure 5.3. The areas addressed in the Conceptual Landscape Master Plan include Imperial Highway, Puente Street, Arovista Avenue, the Mariner Street Extension, "A" Street, project entries, parking areas and the railroad spur.

5.3.2 Street Treatments

The landscaping treatment of the peripheral and internal streets is a vital component to the overall Land Use Plan. The street treatment sections schematically illustrate the relationships of street elements such as landscaping, sidewalks and parkways. Any sculptural mounding within a parkway must be low enough to allow line of sight for vehicles exiting. Mounding will not be permissible near intersections for visibility reasons. The mounding will increase safety along the pedestrian corridors, screen the parking lot, and create a more aesthetically pleasing and enclosed walkway system.

5.3.3 Project Entries and Monumentation

The landscape concept theme for the entries is designed to create a sense of arrival into the project at various locations. These entries will be differentiated by a variation in the size, scale and type of plant material selected for each.

The intent of the signage guidelines is to establish an identity for the project and to provide a coordinated graphic system which unifies the overall project identity. Detailing and style shall relate to the architectural character of the project. Monument walls are proposed for each side of the entry drive at "A" Street at the intersection of Imperial Highway. No signage shall be allowed in the public right-of-way. Monument walls shall incorporate muted tones, with variations of white or earth tones. Bright colors shall be used sparingly for accentuation of lettering and trim pieces. All proposed color schemes shall be designed to relate to other signs, graphics, and color schemes in the vicinity. Plant materials shall be designed to enhance and backdrop project signage. Primary and secondary entries will be differentiated by a variation in the number of accent trees used.

5.3.4 Landscape Details and Cross Sections

Landscape details and cross sections have been prepared for various locations throughout the Specific Plan area, as shown on Figure 5-3-1. The landscape concept for each detail and cross section, Art in Public Places locations and plan views, and common outdoor plaza areas between various buildings are depicted are located on Figures 5-3-2 through 5-3-18.



Conceptual Landscape Plan
BREA INDUSTRIAL
 SPECIFIC PLAN

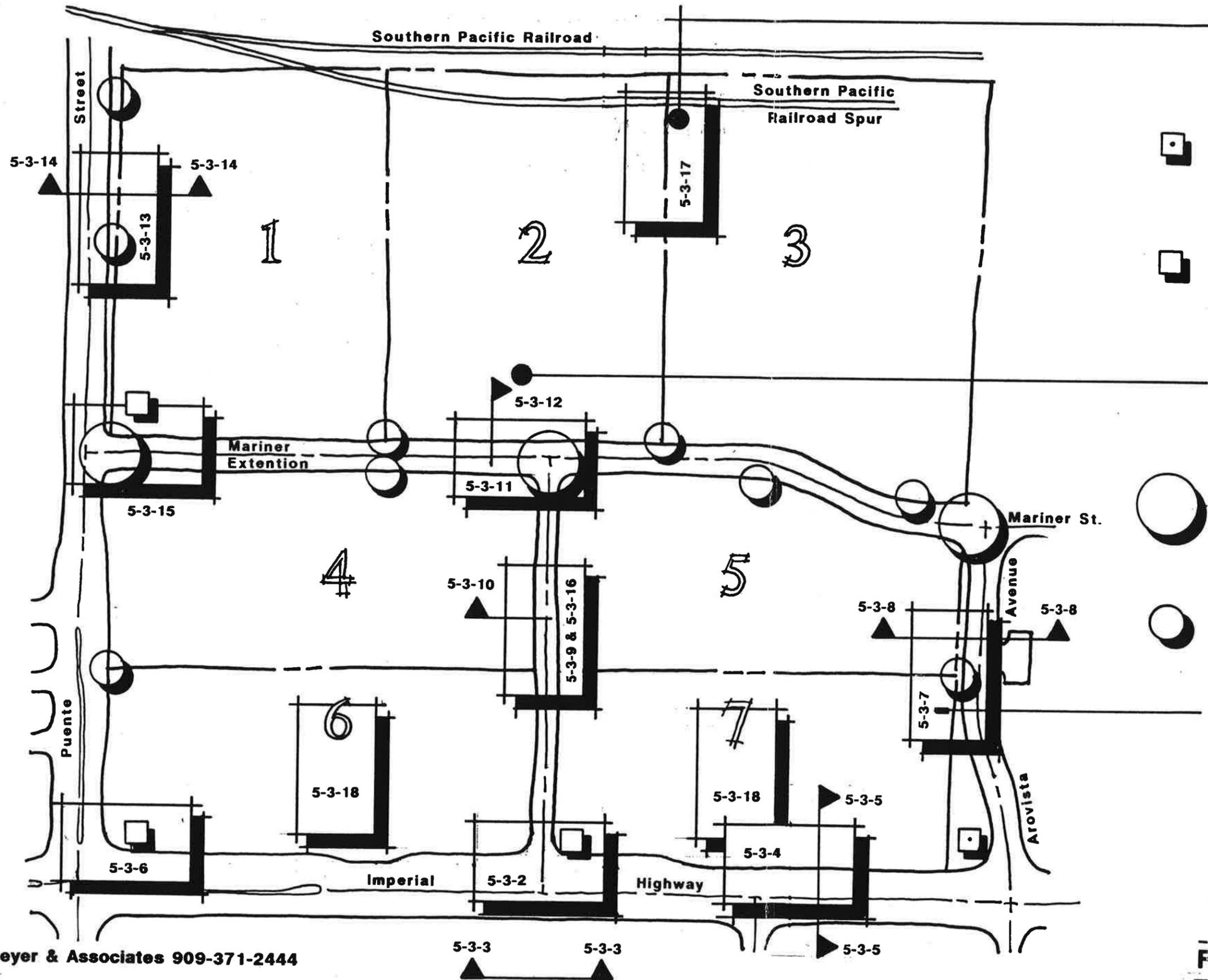
10-5-1999



FIGURE 5-3-0

KAMMEYER & ASSOCIATES

ENVIRONMENTAL DESIGN • LANDSCAPE ARCHITECTURE • PLANNING
 THE RANCH • 2817 KELLOGG AVENUE • CORONA, CA 91719
 (909) 371-2444 FAX (909) 371-4719



INDICATES PLAN VIEW
DETAIL LOCATION
TYPICAL

INDICATES EXISTING
ART IN PUBLIC
PLACES LOCATION

INDICATES PROPOSED
ART IN PUBLIC
PLACES LOCATION

INDICATES SECTION
LOCATION and
DIRECTION

INDICATES SECOND-
ARY ENTRY
LOCATIONS

INDICATES PARCEL
ENTRY LOCATIONS

INDICATES FIGURE
REFERENCE NUMBER

CONCEPTUAL
LANDSCAPE PLAN

NOT TO SCALE

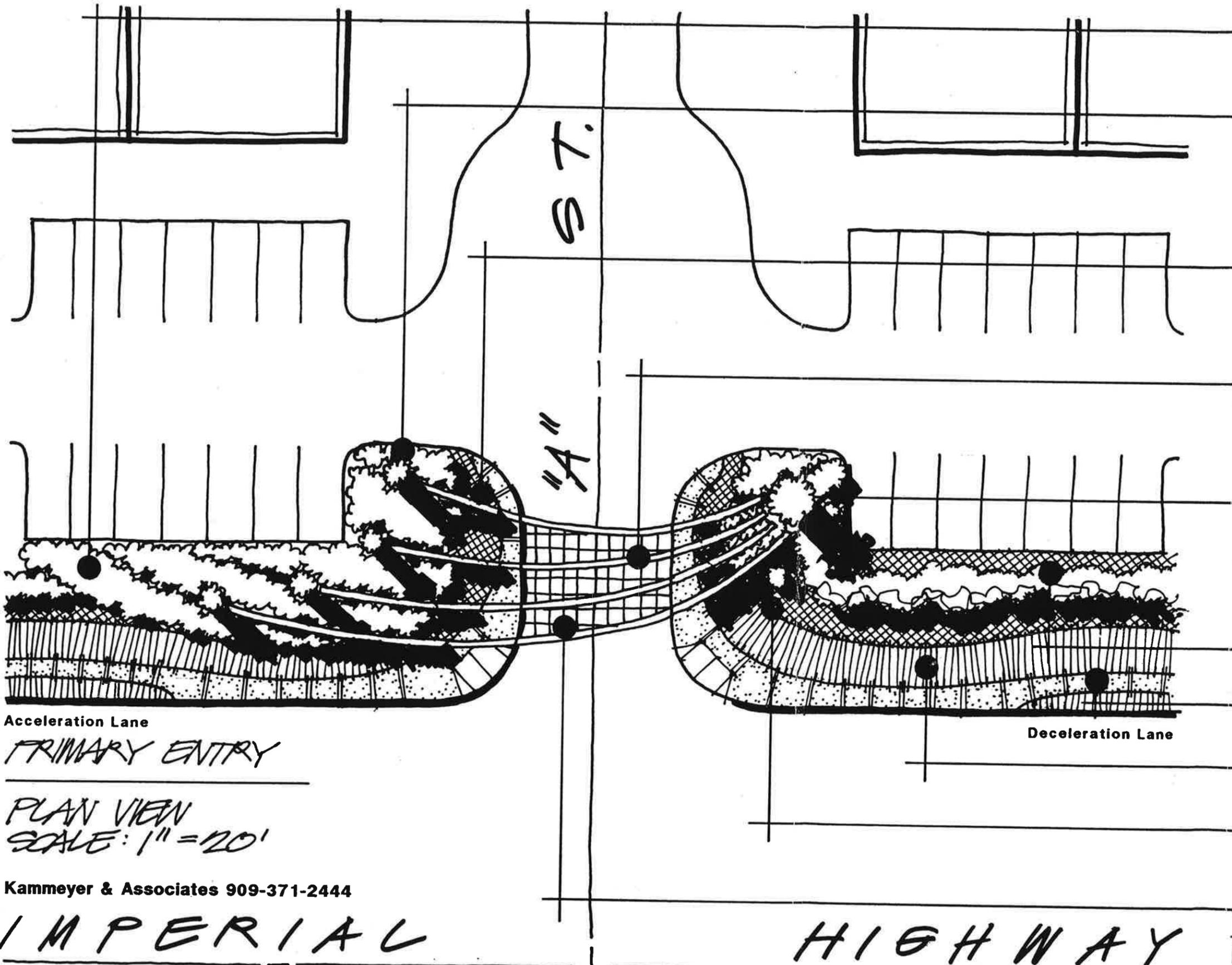
FIGURE 5-3-1

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**BREA INDUSTRIAL
SPECIFIC PLAN**



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- COLORFUL SHRUB BANDS
- 4' DIA. X 10' HIGH MAX. BLOCK COLUMNS W/ FLOWER POTS ABOVE
- COLORFUL BLOCK ENTRY WALLS @ VARYING HEIGHTS
- COLORFUL CONCRETE BANDS THROUGH ROADWAY AND WALKS
- 8' DIA. X 12' HIGH BLOCK ENTRY COLUMN W/ FLOWER POTS ABOVE
- GROUND COVER and SHRUB BAND
- MEANDERING WALK
- TURF EDGE
- "ART IN PUBLIC SPACE" ART PIECE LOCATION
- DECORATIVE PAVING

Acceleration Lane
PRIMARY ENTRY

Deceleration Lane

PLAN VIEW
SCALE: 1" = 20'

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IMPERIAL

HIGHWAY

FIGURE 5-3-2

BREA INDUSTRIAL
SPECIFIC PLAN

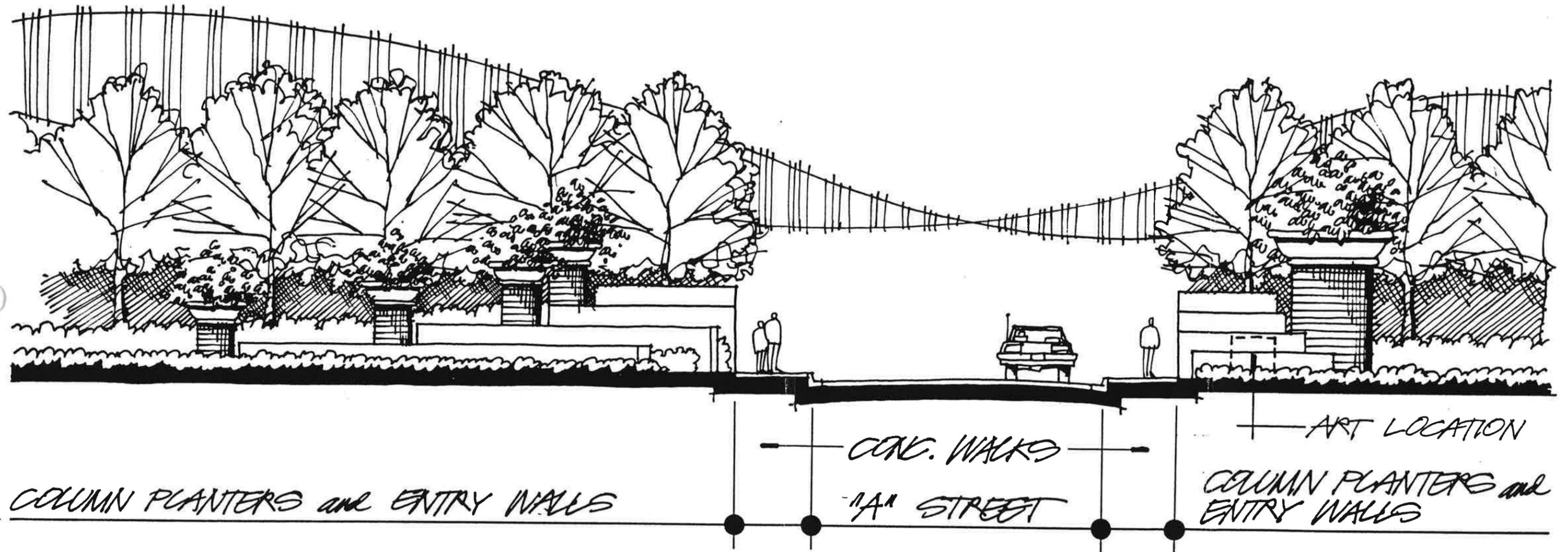


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Inland Empire Division

N:\31384.000\DWG\SP1\FIG5-1.DWG JULY 1998

PRIMARY ENTRY

ELEVATION
SCALE: 1" = 10'



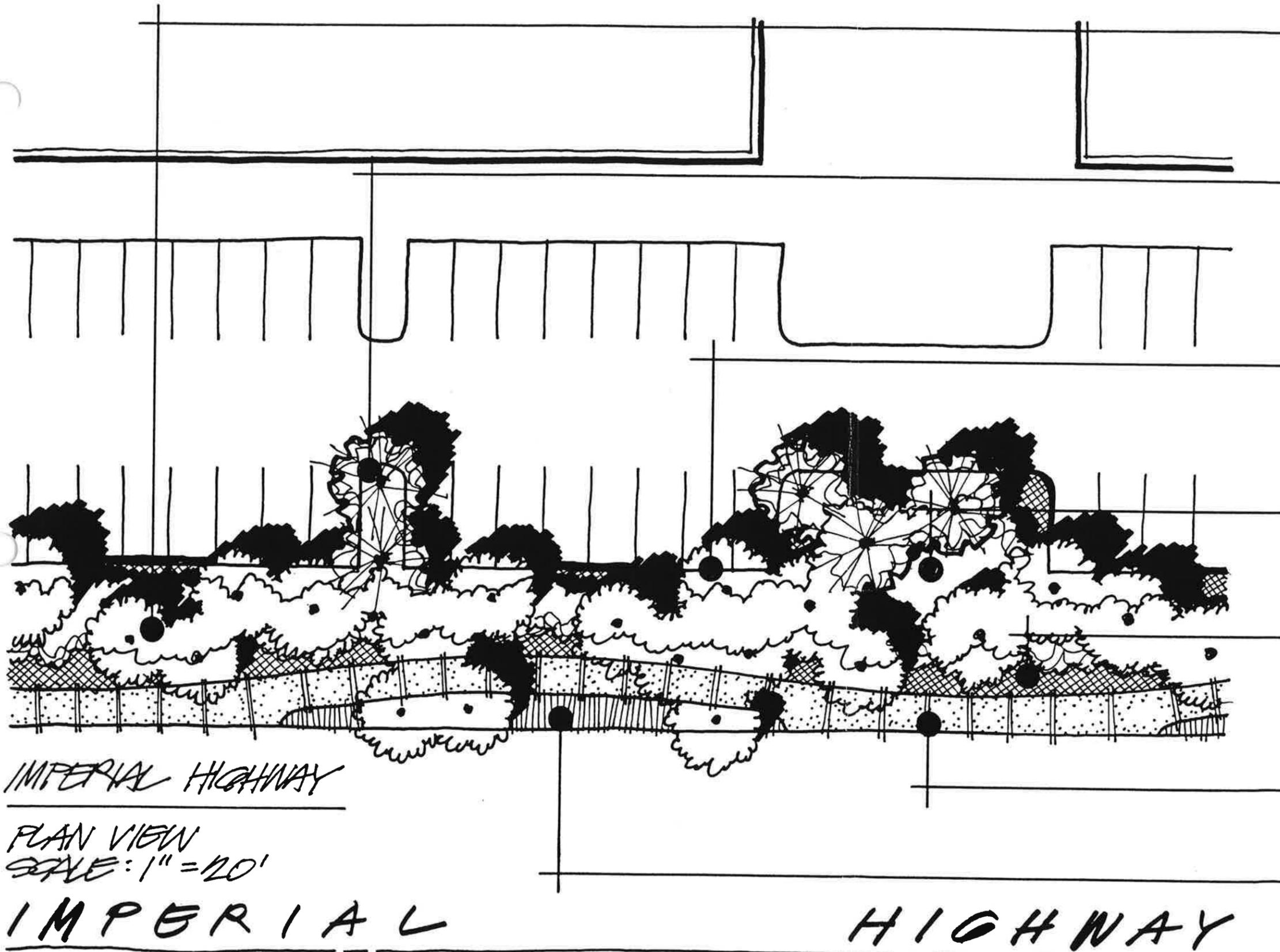
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BREA INDUSTRIAL
SPECIFIC PLAN

FIGURE 5-3-3

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Inland Empire Division

N:\31384.000\DWG\SP1\FIGS-LDWG JULY 1998



INDICATES EUCALYPTUS
TEROQUATA - CORAL
GUM MASS PLANTINGS

INDICATES RHUS
LANCEA - AFRICAN
SUMAC and PISTACHIA
CHINENSIS - CHINESE
PISTACHE IN PARKING
LOT TYPICAL

INDICATES LORHO -
STEMON CONFERTUS -
BRISBANE BOX
MASS PLANTING

INDICATES LIQUSTRUM
JAPONICUM 'TEXANUM'
SHRUB MASSES

INDICATES TRACHELO-
SPERMUM JASMIN -
OIBS - STAR JAS-
MINE GROUND COVER

NEW MEANDERING
CONCRETE WALK

INDICATES TURF

IMPERIAL HIGHWAY

PLAN VIEW
SCALE: 1" = 10'

IMPERIAL

HIGHWAY

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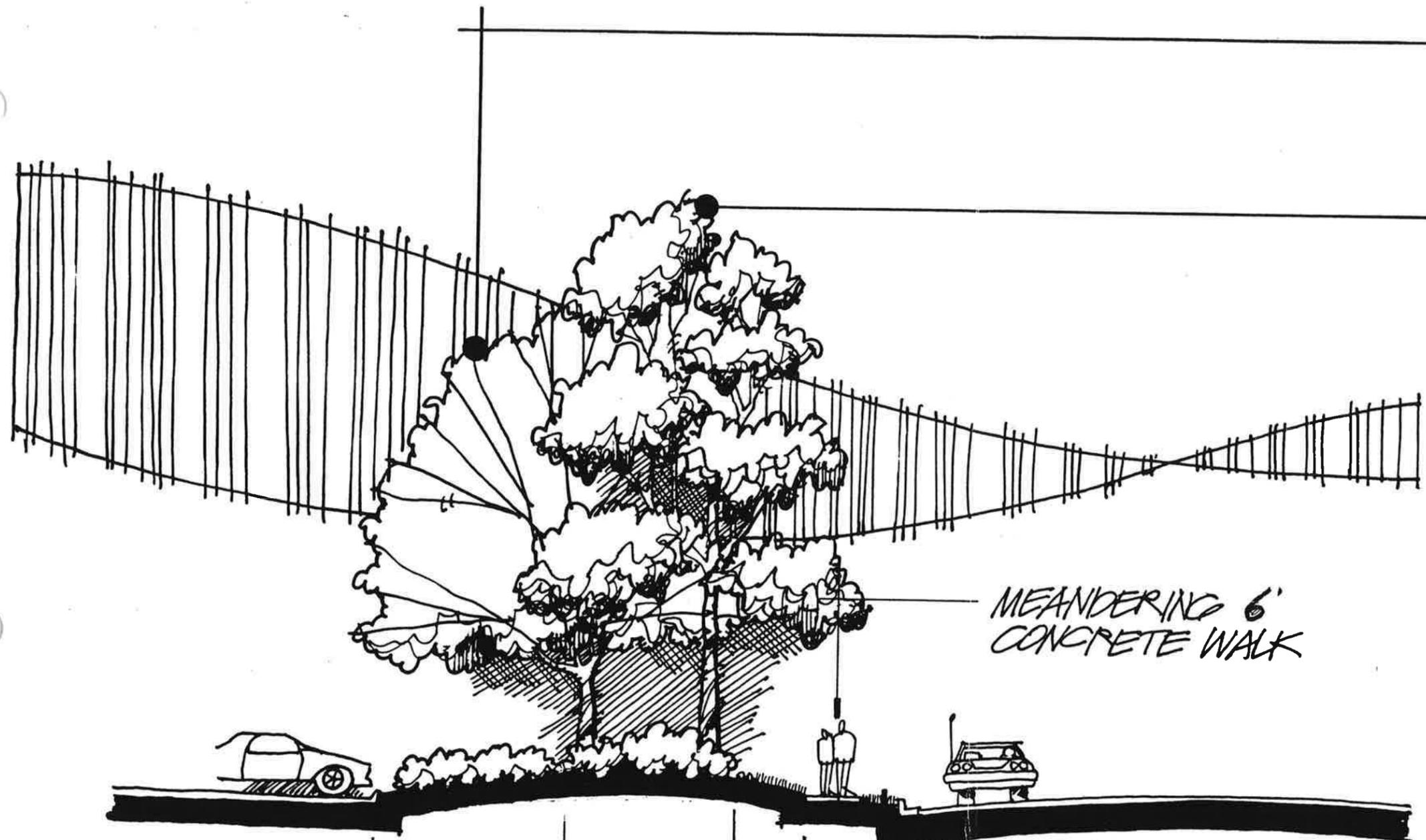
BREA INDUSTRIAL
SPECIFIC PLAN

FIGURE 5-3-4



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Inland Empire Division

N:\31384.000\DWG\SP1\FIG5-1DWG JULY 1998



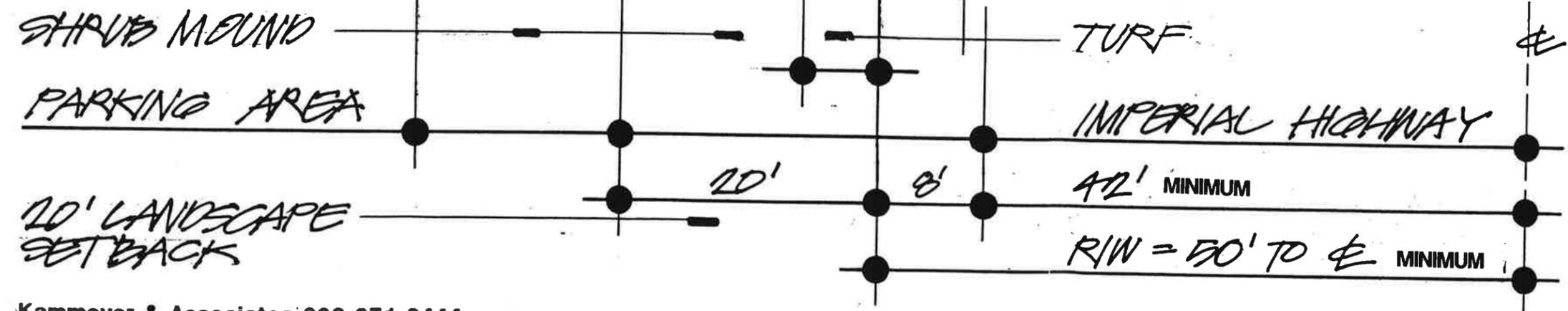
INDICATES LOPHOSTEM-
ON CONPERTUS -
BRISBANE BOX MASS
PLANTINGS

INDICATES EUCALYPTUS
TORQUATA - CERAL
CUM MASS TREE
PLANTINGS

• SHRUB MASSES:
LIQUSTRUM JAPEN-
ICUM 'TEXANUM'

• GROUND COVER:
TRACHELOSPERMUM
JASMINOIDES -
STAR JASMINE

MEANDERING 6'
CONCRETE WALK



IMPERIAL HIGHWAY

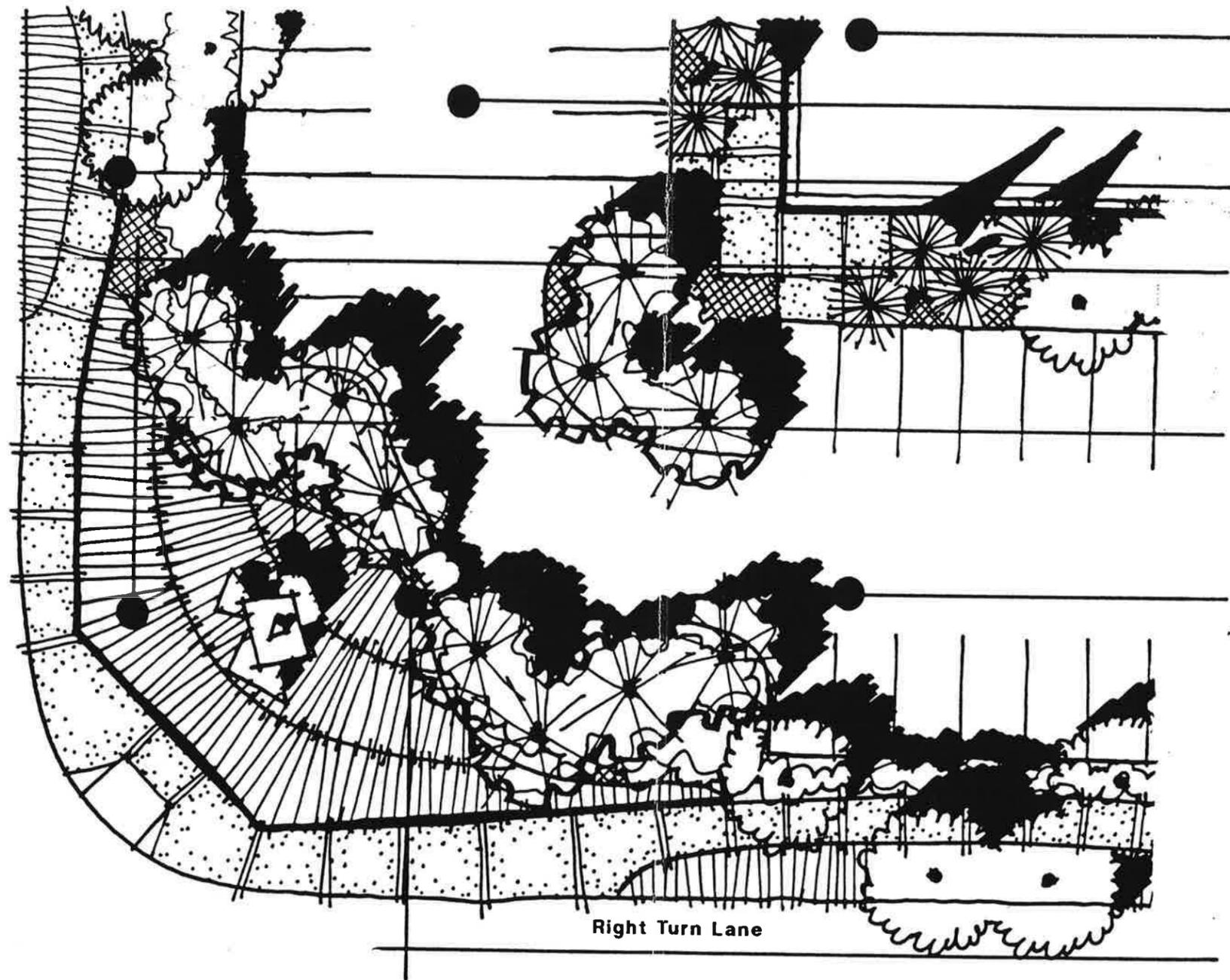
ELEVATION
SCALE: 1" = 10'

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**BREA INDUSTRIAL
SPECIFIC PLAN**

FIGURE 5-3-5

PVENTE STREET



- BUILDING
- PARKING AREA TYP.
- CONCRETE WALK
MEANDERING
- TURF FOREGROUND
and BACKGROUND
FOR ART PIECE
- 'ART IN PUBLIC
SPACES' ART
PIECE
- PLANTING PER FIG.
D.M.A

SHRUB - LIQUISTRUM
JAPONICUM 'TEXANUM' -
BACKDROP FOR
ART PIECE

IMPERIAL HIGHWAY

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BREA INDUSTRIAL
SPECIFIC PLAN

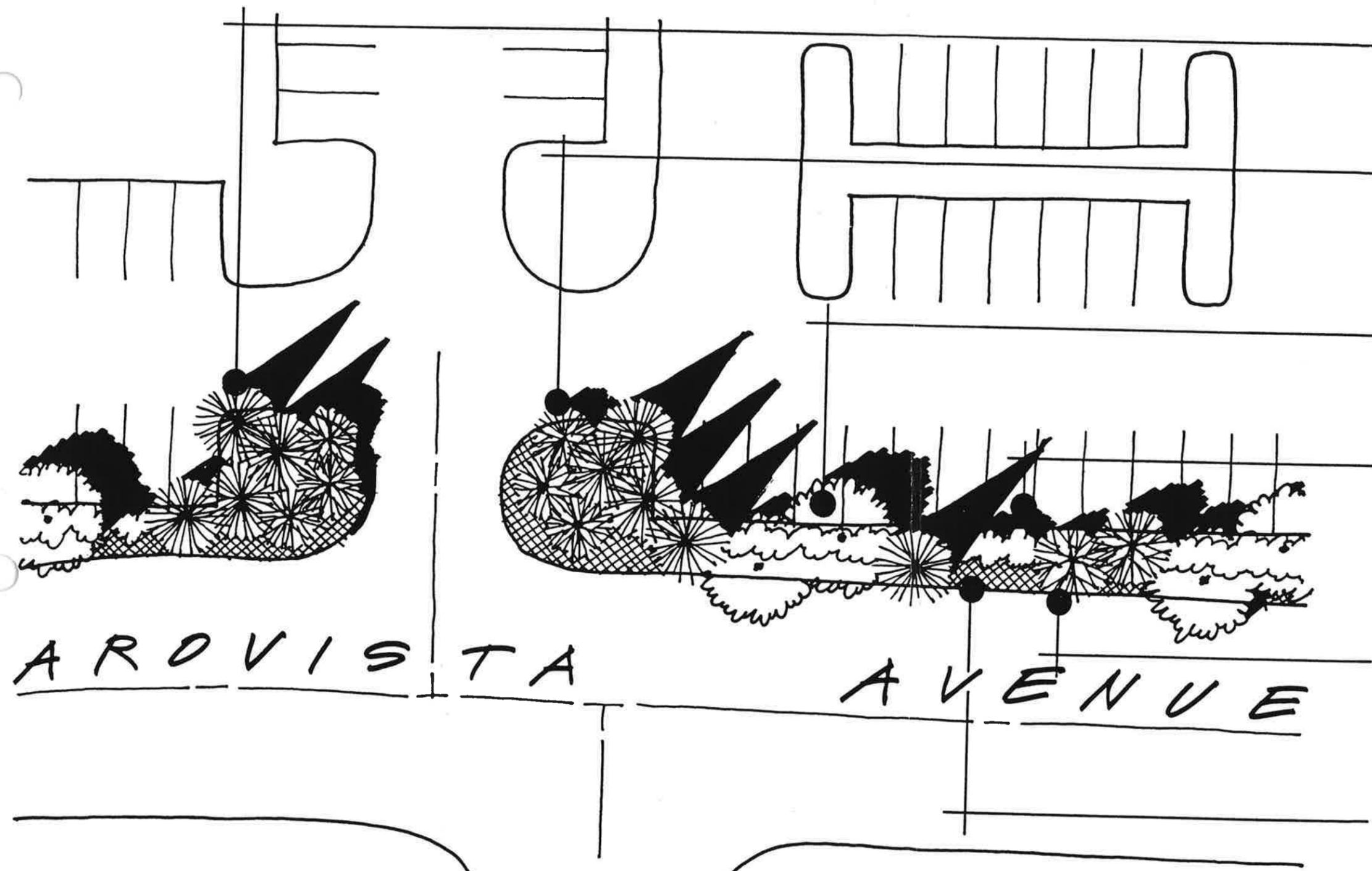
ART LOCATION DETAIL @
IMPERIAL HIGHWAY and PVENTE
PLAN VIEW - SCALE: 1" = 20'

FIGURE 5-3-6



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Inland Empire Division

N:\31284.000\DWG\SPT\FIGS-1.DWG JULY 1998



INDICATES PINUS
CANARIENSIS -
CANARY ISLAND PINE

INDICATES LAGER-
STROEMIA INDICA
'WATERMELON RED'
GRAPE MYRTLE

INDICATES GINKGO
BILOBA - MAIDENHAIR
TREE

INDICATES EUCALYPTUS
RUBRA 'C.F. BALL'

INDICATES LIQUID-
AMBAR STYRACIFLUA
'ROTUNDOLOBA' -
AMERICAN SWEET GUM

INDICATES TRACHELO-
SPERMUM JASMIN-
IODES - STAR
JASMINE GROUND
COVER

AROVISTA AVENUE

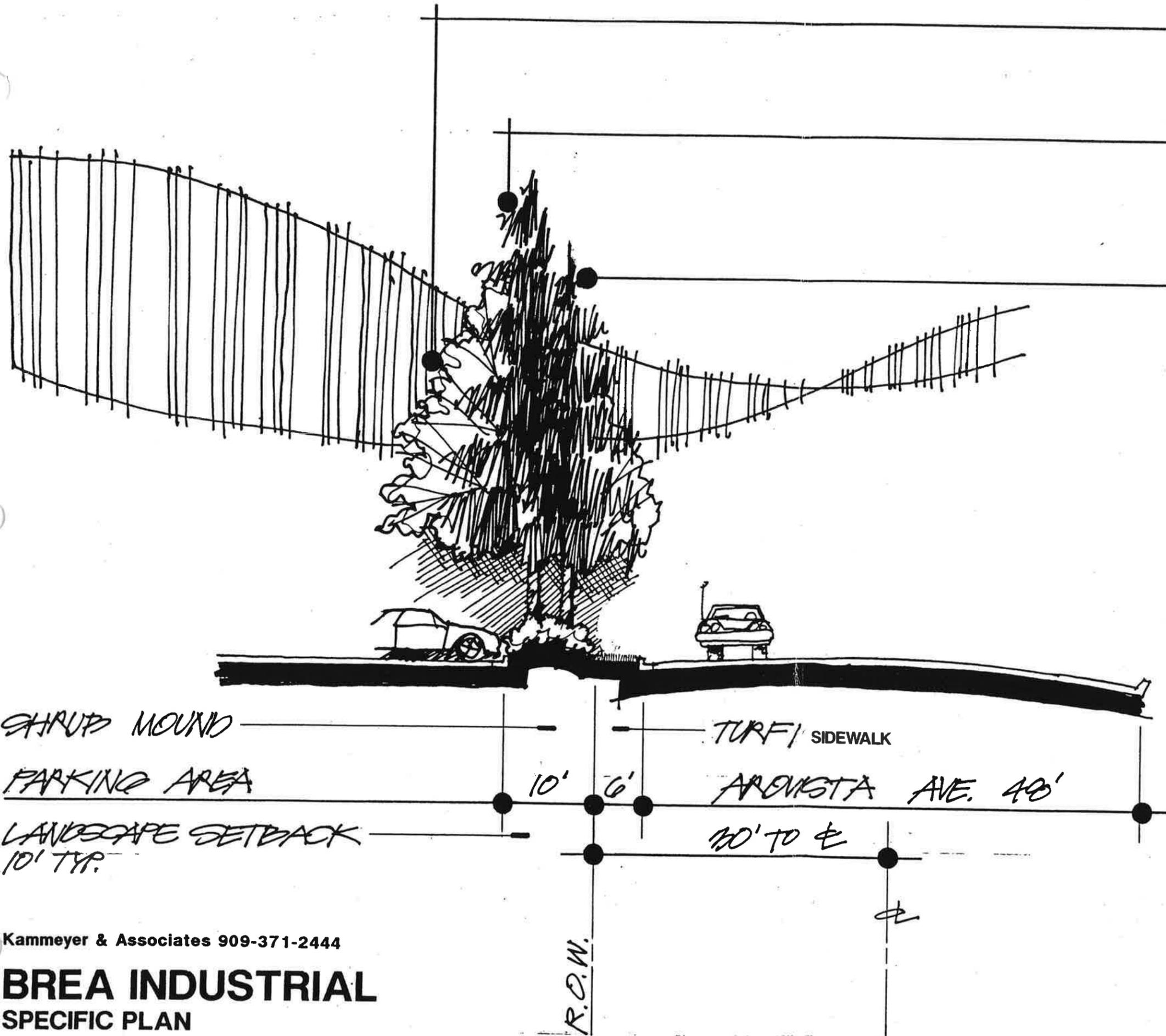
AROVISTA AVENUE

PLAN VIEW
SCALE: 1" = 120'

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BREA INDUSTRIAL
SPECIFIC PLAN

FIGURE 5-3-7



INDICATES GINKGO
BILOBA - MAIDENHAIR-
TREE MASSES

INDICATES PINUS
CANARIENSIS -
CANARY ISLAND
PINE

LIQUIDAMBAR STY-
RACIFLUA 'ROTUNDO-
LOBA' - AMERICAN
SWEET GUM MASSES

• SHRUB MASSES:
EUCALYPTA RUBRA
'O. F. BALL'

• GROUND COVER
TRACHELOSPERMUM
JASMINOIDES - STAR
JASMINE

SHRUB MOUND

PARKING AREA

LANDSCAPE SETBACK
10' TYP.

TURF/ SIDEWALK

ROADWAY AVE. 48'

10' TO ±

R.O.W.

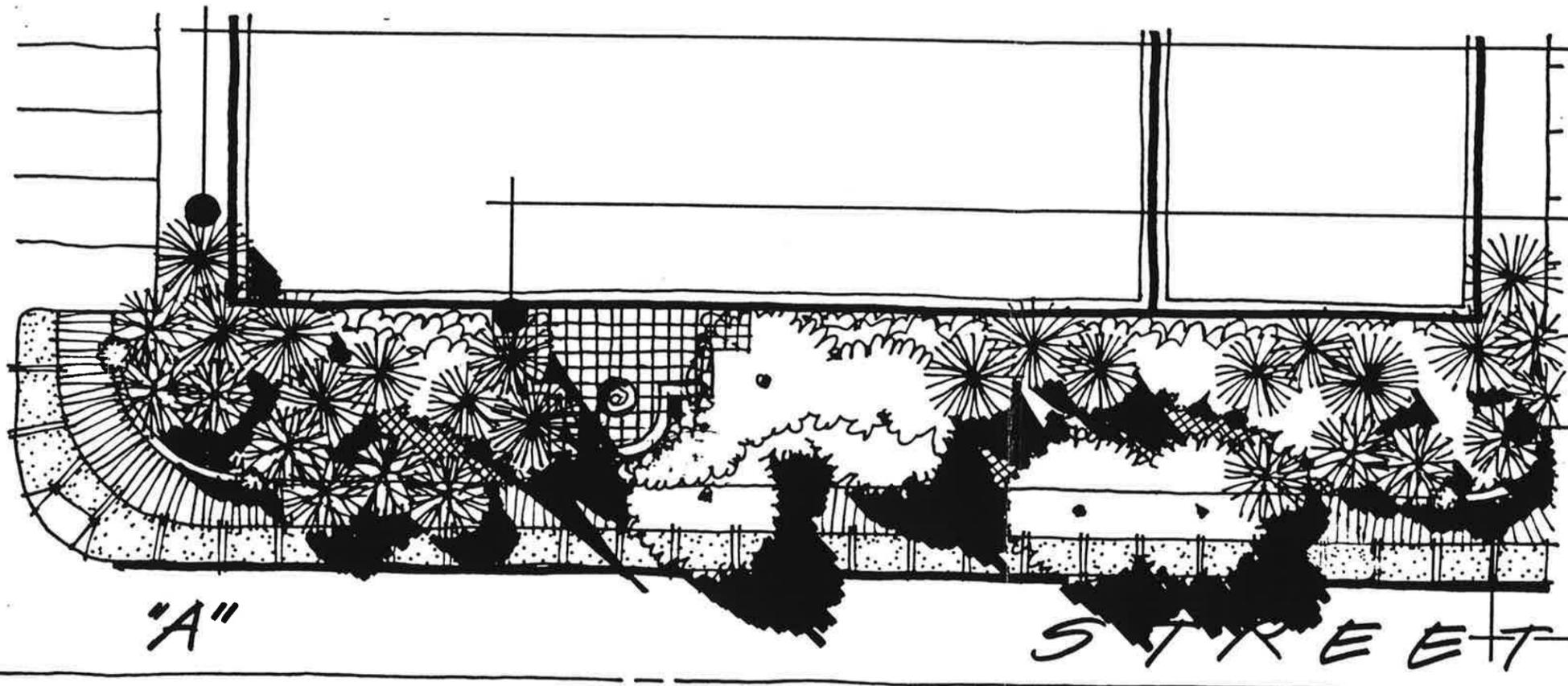
ROADWAY AVENUE

ELEVATION
SCALE: 1" = 10'

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**BREA INDUSTRIAL
SPECIFIC PLAN**

FIGURE 5-3-8



INDICATES PINUS
CANARIENSIS -
CANARY ISLAND PINE

LIQUIDAMBAR STY-
RACIFLUA 'ROTUNDO-
LOBA' - AMERICAN
SWEET GUM MASSES

INDICATES LADER-
STROEMIA INDICA
'WATERMELON RED'
GRAPE MYRTLE

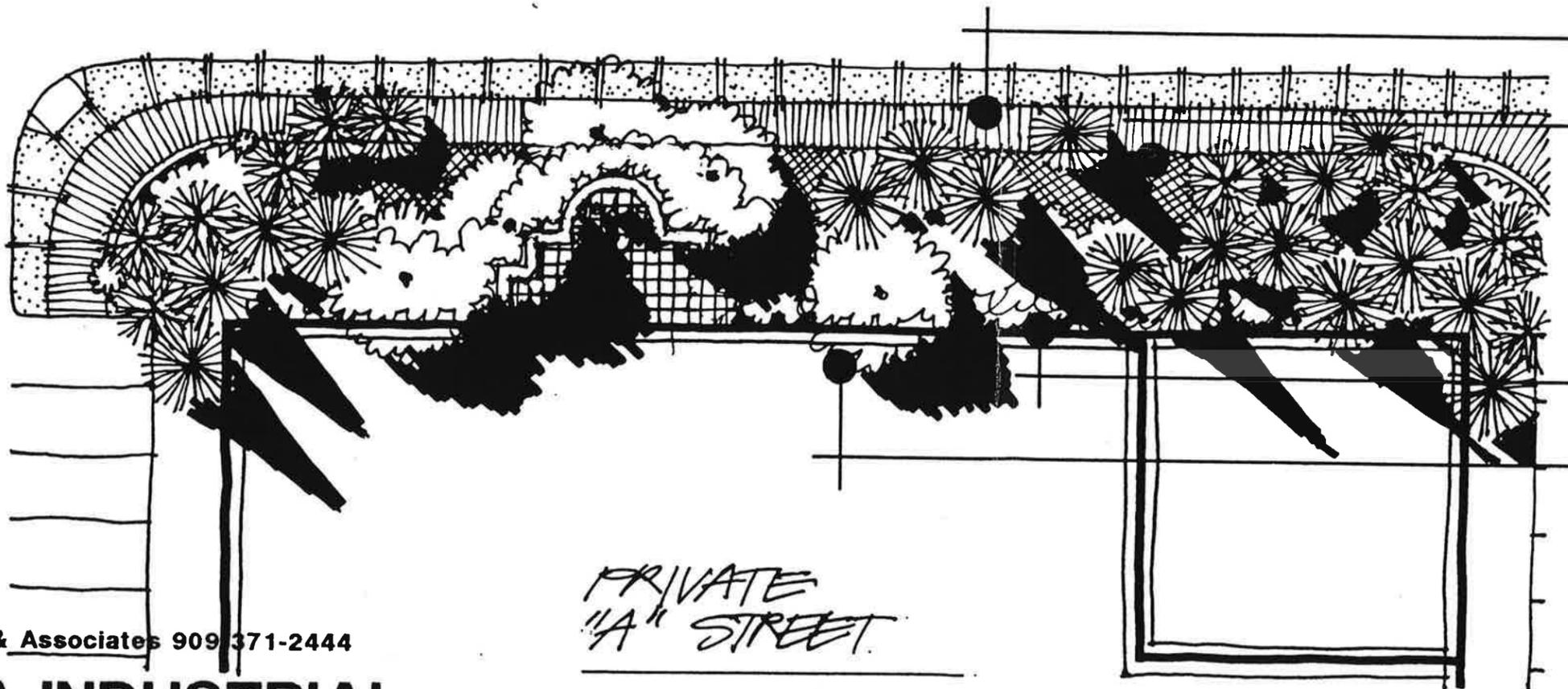
INDICATES PARCEL
BOUNDARY

INDICATED TURF

INDICATES TRACHELO-
STEMMUM JASMINIODES
STAR JASMINE GROUND
COVER PLANTING

INDICATES EBENALONIA
RUBRA 'C.P. BALL'

INDICATES GINKGO
BILoba - MAIDENHAIR
TREE MASSES



PRIVATE
"A" STREET

PLAN VIEW
SCALE: 1" = 10'

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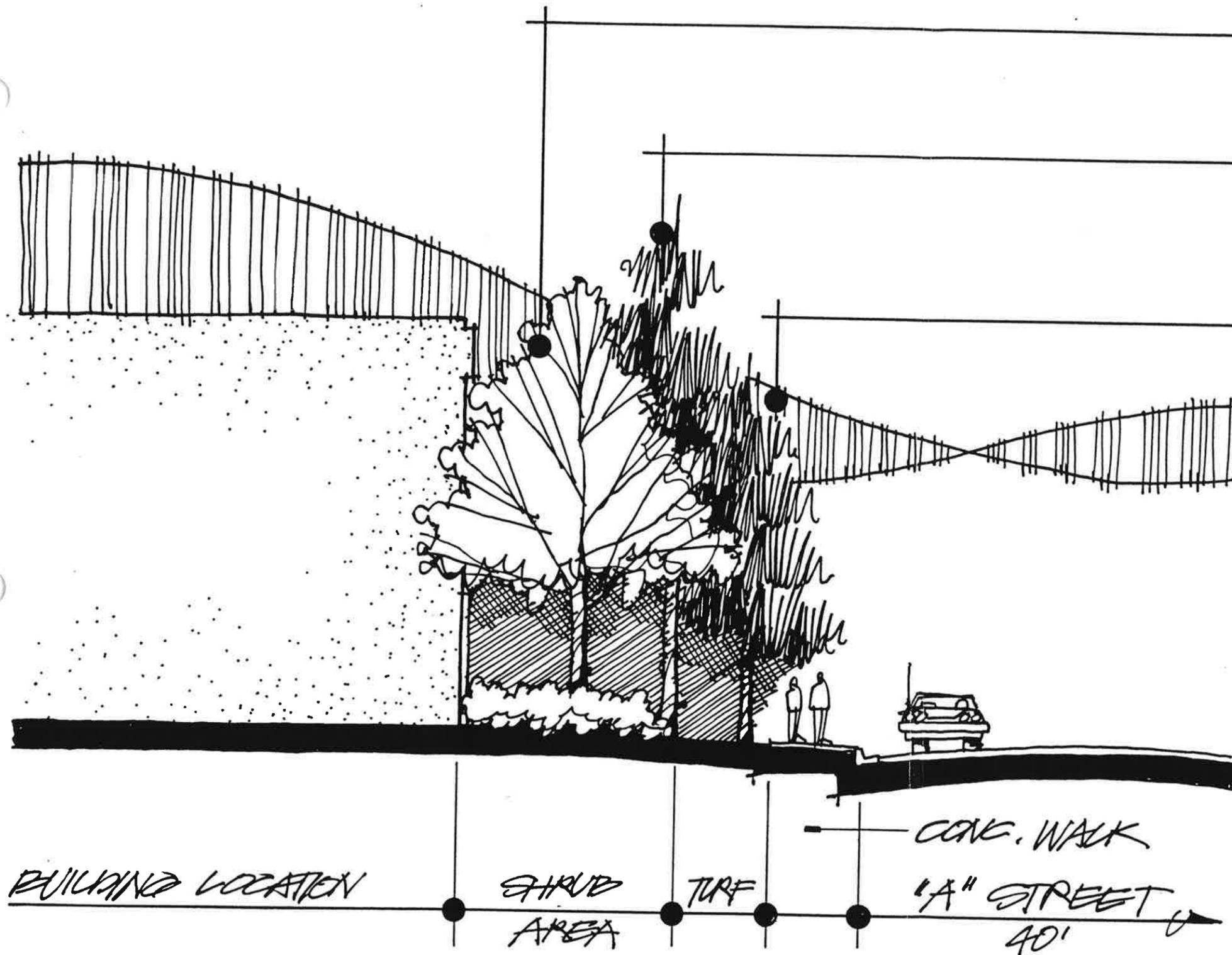
**BREA INDUSTRIAL
SPECIFIC PLAN**

FIGURE 5-3-9



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Inland Empire Division

N:\31384.000\DWG\SP1\FIG6-LDWG JULY 1998



INDICATES GINKGO
BILOBA - MAIDEN-
HAIR TREE MASSES

INDICATES PINES
CANARIENSIS -
CANARY ISLAND
PINE

LIQUIDAMBAR STY-
RACIFLUA 'ROTUNDO-
LOBA' - AMERICAN
SWEET GUM MASSES

• SHRUB MASSES:
ESCALONIA RUBRA
'C.F. BALL' ©
BUILDING EDGE

• GROUND COVER:
TRACHELOSPERNUM
JASMINOIDES -
STAR JASMINE

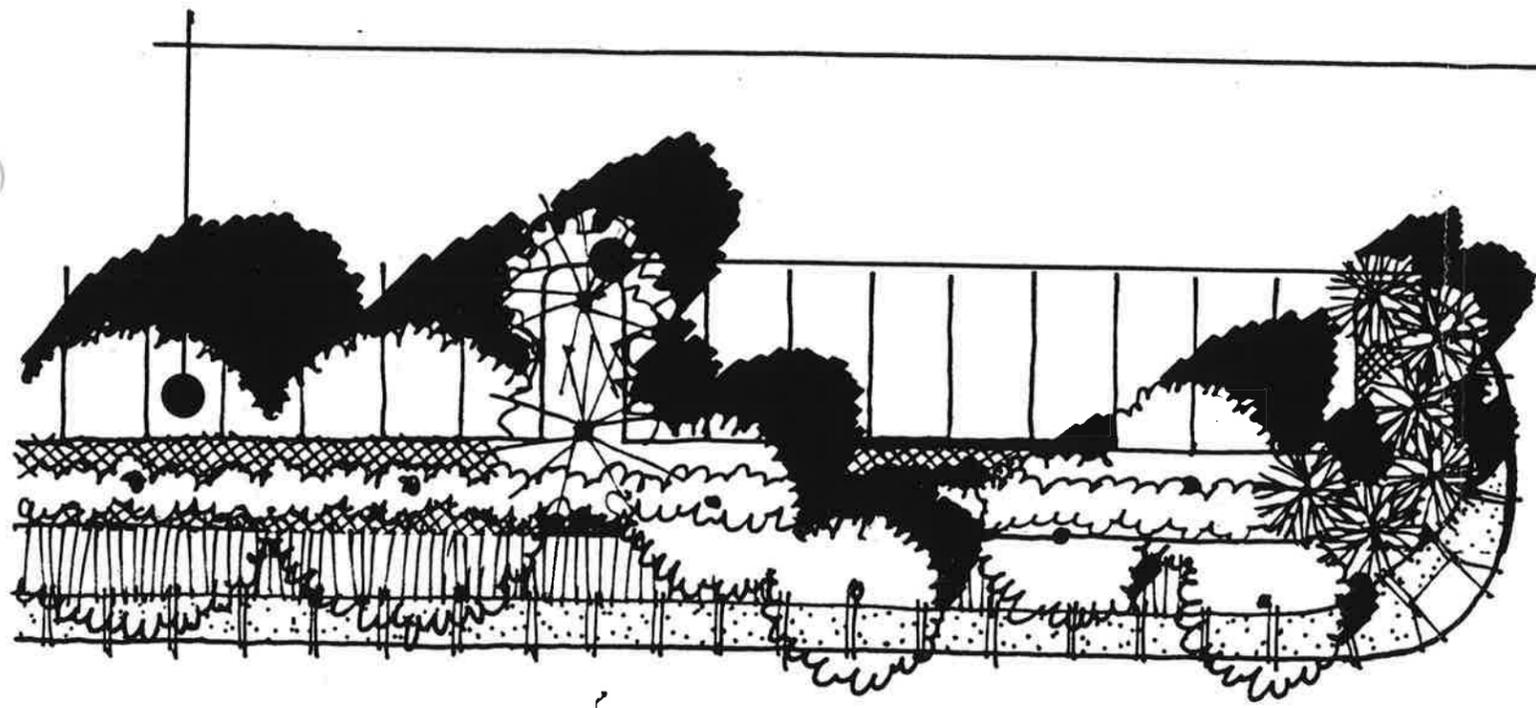
PRIVATE
"A" STREET

ELEVATION
SCALE: 1" = 10'

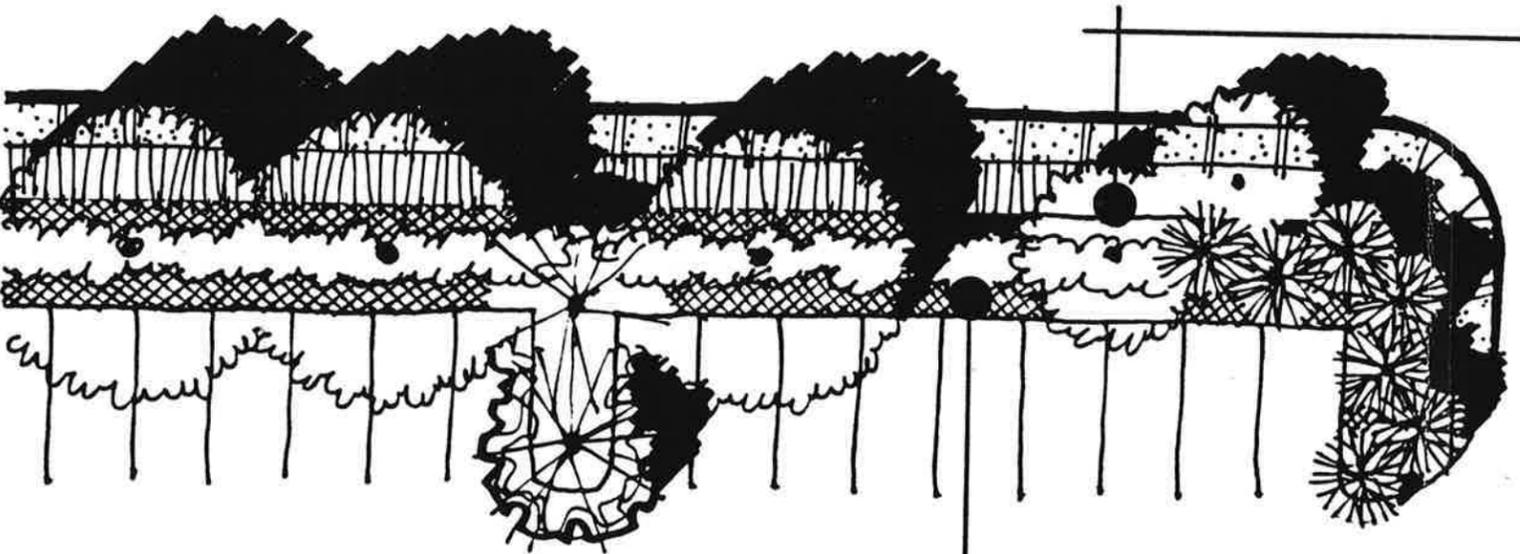
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BREA INDUSTRIAL
SPECIFIC PLAN

FIGURE 5-3-10



MARINER EXTENSION



MARINER EXTENSION

PLAN VIEW
SCALE: 1" = 20'

INDICATES EUCALYPTUS
LEUCOXYLON - WHITE
IRONBARK MASSES

INDICATES RHUS
LANCEA - AFRICAN
SUNAC and PISTACIA
CHINENSIS - CHINESE
PISTACHE IN PARKING
LOT TYPICAL

INDICATED TURF

INDICATES CALLIST-
EMEN VIMINALIS -
WEeping BUTLEBRUSH
ACCENT TREES

INDICATED GELERA
PARVIFLORA - AUST-
RALIAN WILLOW
MASSES

INDICATED TRACHELO-
SPERMUM JASMINOIDES -
STAR JASMINE
GROUND COVER

INDICATES RHAPHIOLEPS
INDICA 'SPRINGTIME'
SHRUB MASS

FIGURE 5-3-11

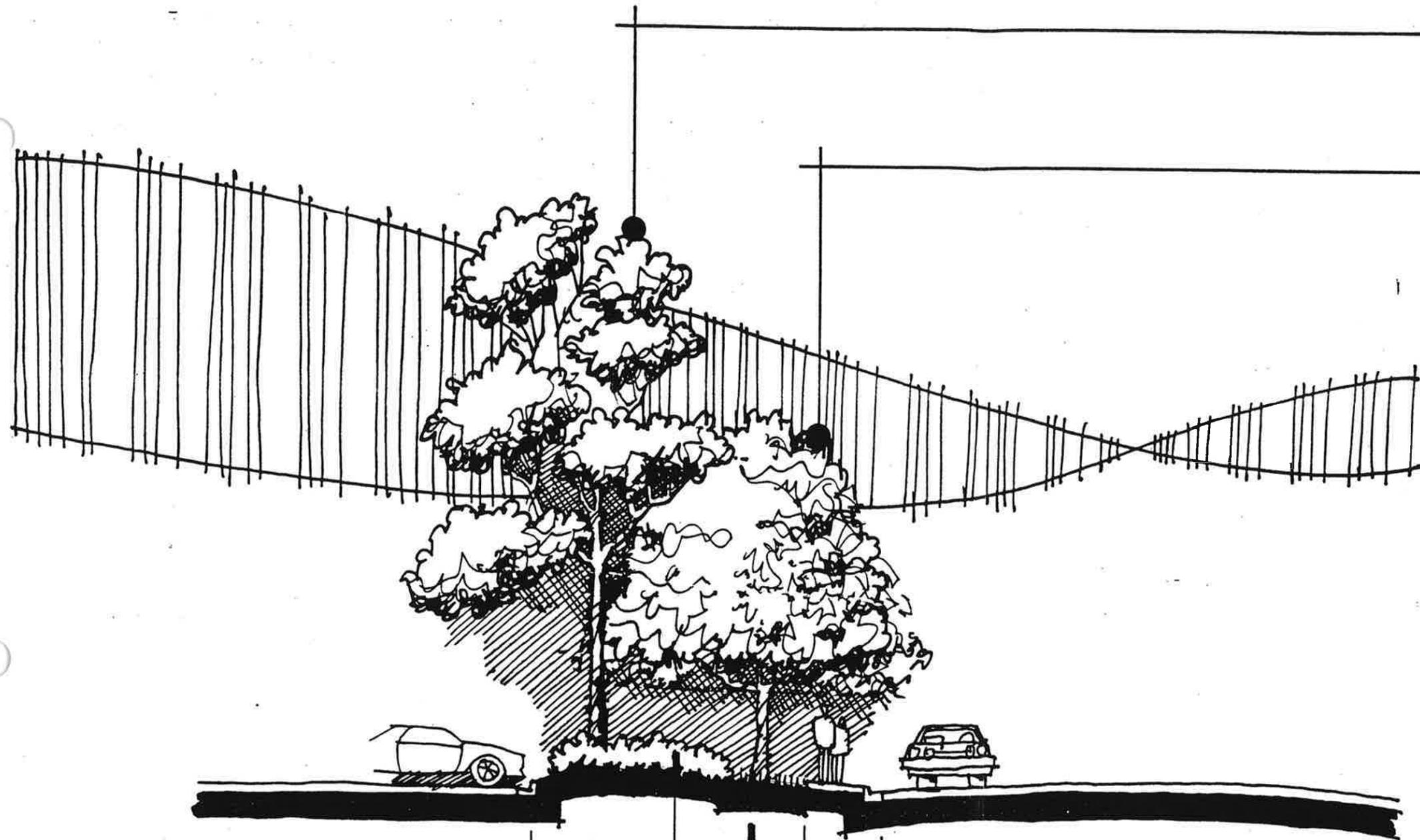
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BREA INDUSTRIAL
SPECIFIC PLAN



The Keith Companies
Inland Empire Division

R:\3284.000\DWG\SP11\FIGS-1.DWG JULY 1998



INDICATES EUCALYPTUS
LEUCOXYLON - WHITE
IRONBARK MASSES

INDICATES GENERA
PARVIFLORA - AUST-
RALIAN WILLOW
MASSES

• SHRUB MASSES:
RHAPHIOLEPTIS INDICA
'SPRINGTIME' SHRUB
MASSES

• GROUND COVER:
TRACHELOSTERNUM
JASMINOIDES -
STAR JASMINE

SHRUB MOUND

PARKING AREA

LANDSCAPE SETBACK

CONCRETE WALK
TURF BAND

6'

400' - CURBS TO CURBS

MARINER EXTENSION

ELEVATION
SCALE: 1" = 10'

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BREA INDUSTRIAL
SPECIFIC PLAN

FIGURE 5-3-12

P U E N T E

S T R E E T

INDICATES GINKGO
BILoba - MAJENHAIR
TREE MASSES

INDICATES PINUS
CANARIENSIS -
CANARY ISLAND PINE

LIQUIDAMBAR STY-
RACIFLUA 'ROTUNDO-
LOBA' - AMERICAN
SWEET GUM MASSES

INDICATES PARCEL
SIGNAGE

INDICATES TURF

INDICATES LACER-
STROENIA INDICA
'WATERMELON RED' -
CRATE MYRTLE

INDICATES TRACHELO-
SPERMUM JASMINOIDES
STAR JASMINE GROUND
COVER PLANTING

INDICATES ESCALLONIA
RUBRA 'C.F. BALL'

PUBNTE STREET
PLAN VIEW
SCALE: 1" = 20'

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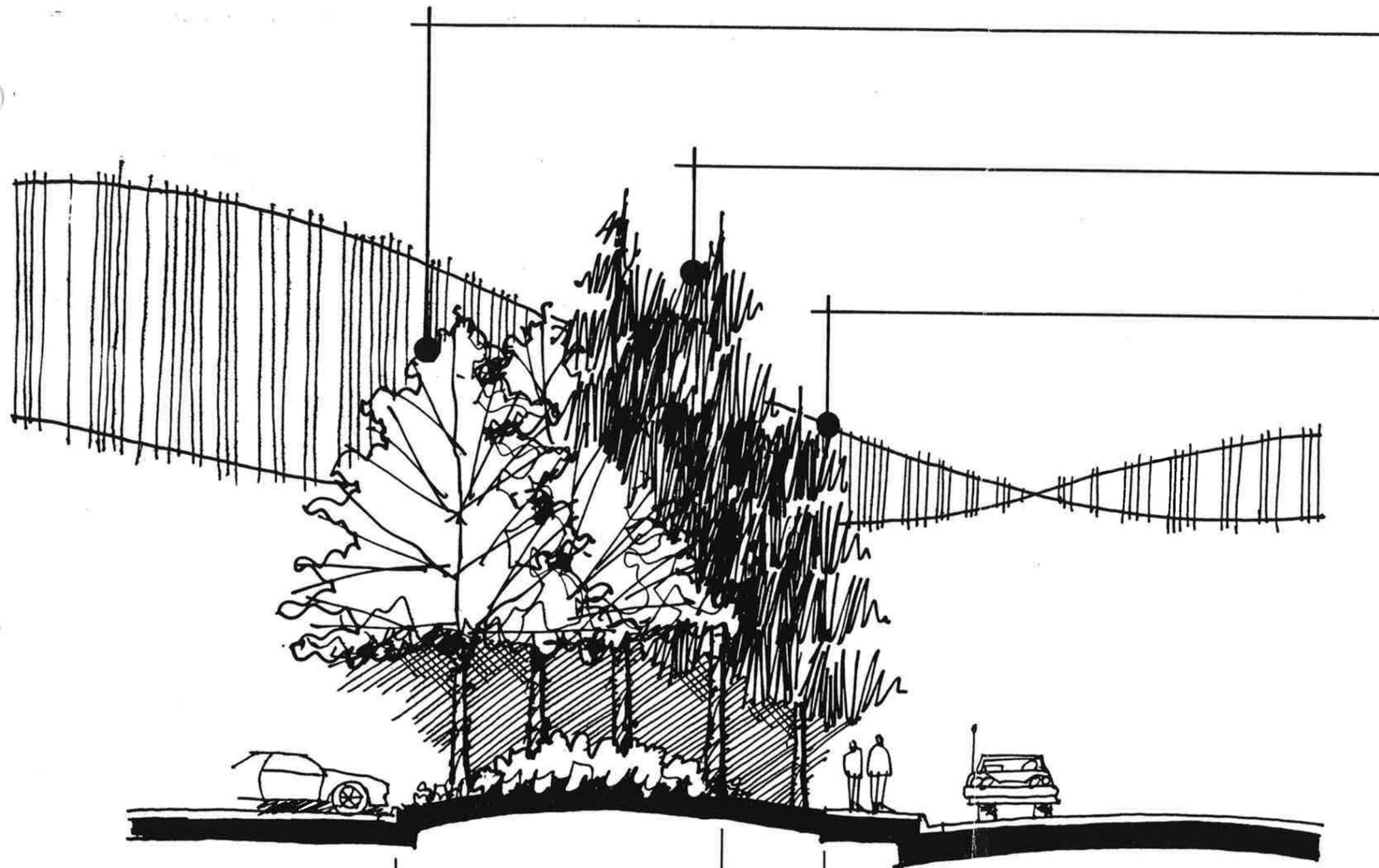
BREA INDUSTRIAL
SPECIFIC PLAN

FIGURE 5-3-13



The Keith Companies
Inland Empire Division

N:\31384.000\DWG\SP1\FIG5-LDWG JULY 1998



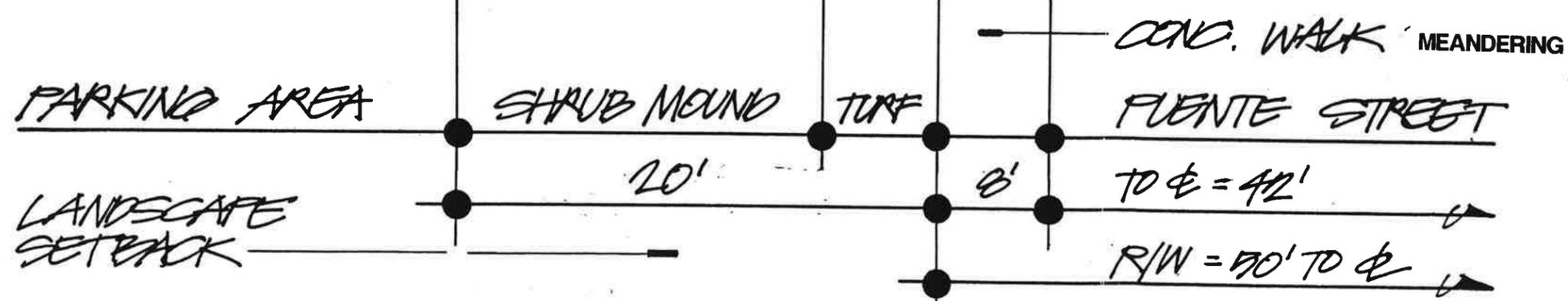
INDICATES QINKGO
 BILOBA - MAIDEN-
 HAIR TREE MASSES

INDICATES PINUS
 CANARIENSIS -
 CANARY ISLAND PINE

LIQUIDAMBAR STY-
 RACIFLUA 'ROTUNDO-
 LOBA' - AMERICAN
 SWEET GUM MASSES

• SHRUB MASSES:
 ESCALLONIA RUBRA
 'O.F. BALL' ON
 MOUND

• GROUND COVER:
 TRACHELOSPERMUM
 JASMINOIDES -
 STAR JASMINE



PUENTE STREET

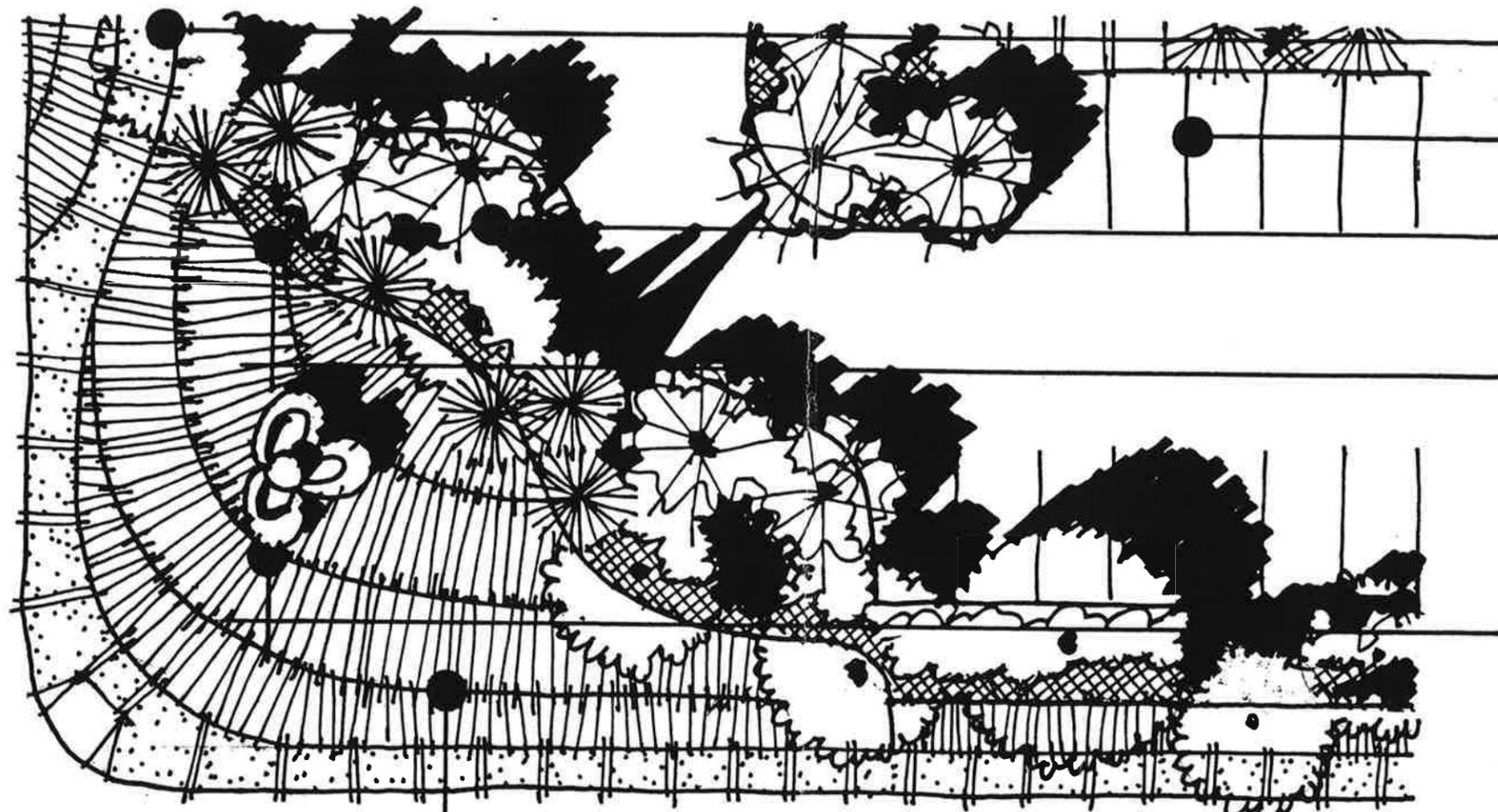
ELEVATION
 SCALE: 1" = 10'

Kammeyer & Associates 909-371-2444

**BREA INDUSTRIAL
 SPECIFIC PLAN**

FIGURE 5-3-14

P U E N T E S T R E E T



CONCRETE WALK

PARKING LOT TYP.

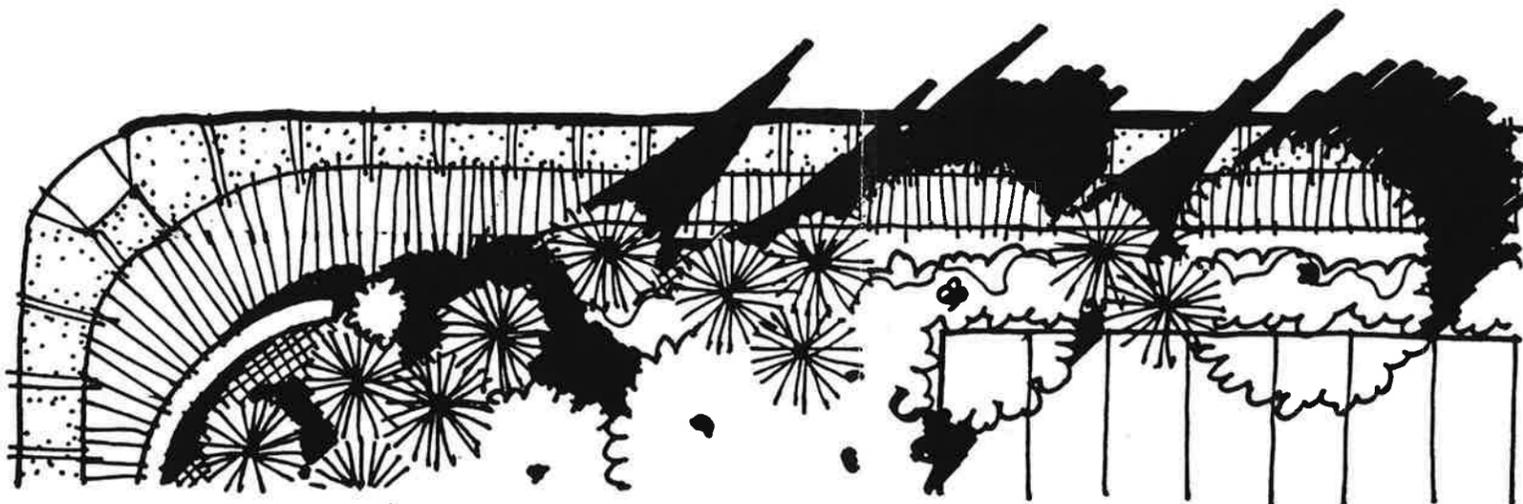
TREES - SEE FIG. 5.3.14

SHRUB - ESCALLONIA RUBRA 'C.F. BALL' - BACKDROP FOR ART PIECE

'ART IN PUBLIC SPACES' ART PIECE

TURF FOREGROUND AND BACKGROUND FOR ART PIECE

MARINER EXT.



ART LOCATION DETAIL @ PUENTE STREET and MARINER EXTENSION

PLAN VIEW SCALE: 1" = 10'

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BREA INDUSTRIAL
SPECIFIC PLAN

FIGURE 5-3-15

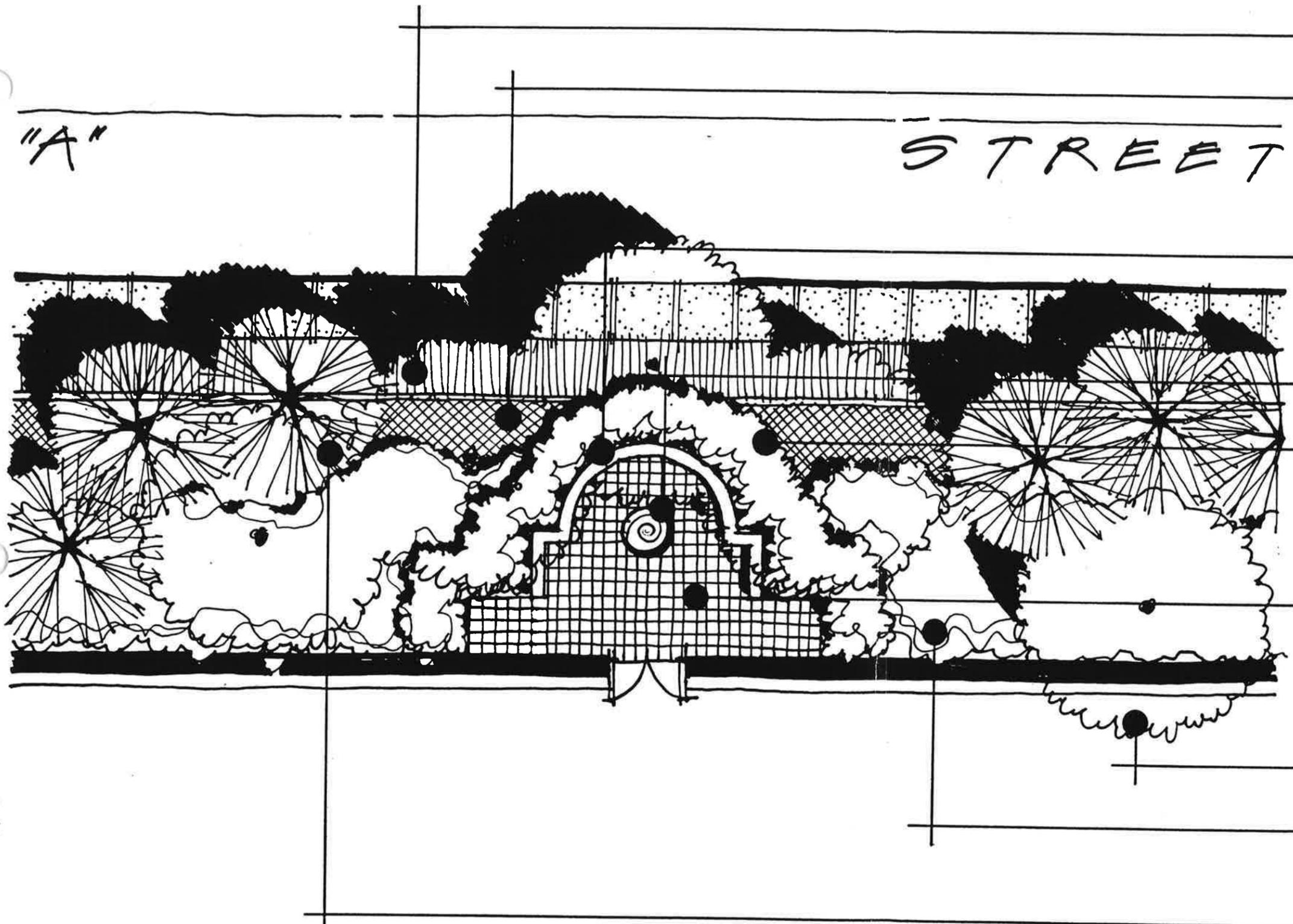


The Keith Companies
Inland Empire Division

N:\31284.000\DWG\SPT\FIG-10WG JULY 1998

"A"

STREET



INDICATES TURF

INDICATES TRACHELO-
SPERMUM JASMINIODES
STAR JASMINE
GROUND COVER

10" HIGH PLASTER
SEATWALL

SMALL REFLECTION
POOL

INDICATES FUMBAGO
AURICULATA - CAFE
FUMBAGO SHRUB
SCREEN

DECORATIVE COURT-
YARD PAVING

INDICATES ENKEO
PILOBA - MAIDENHAIR
TREES MASSES

INDICATES ESCALLENA
RUBRA 'C.F. BALL'

INDICATES LAGERSTRO-
EMIA - CRAFT MYRTLE

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BREA INDUSTRIAL SPECIFIC PLAN

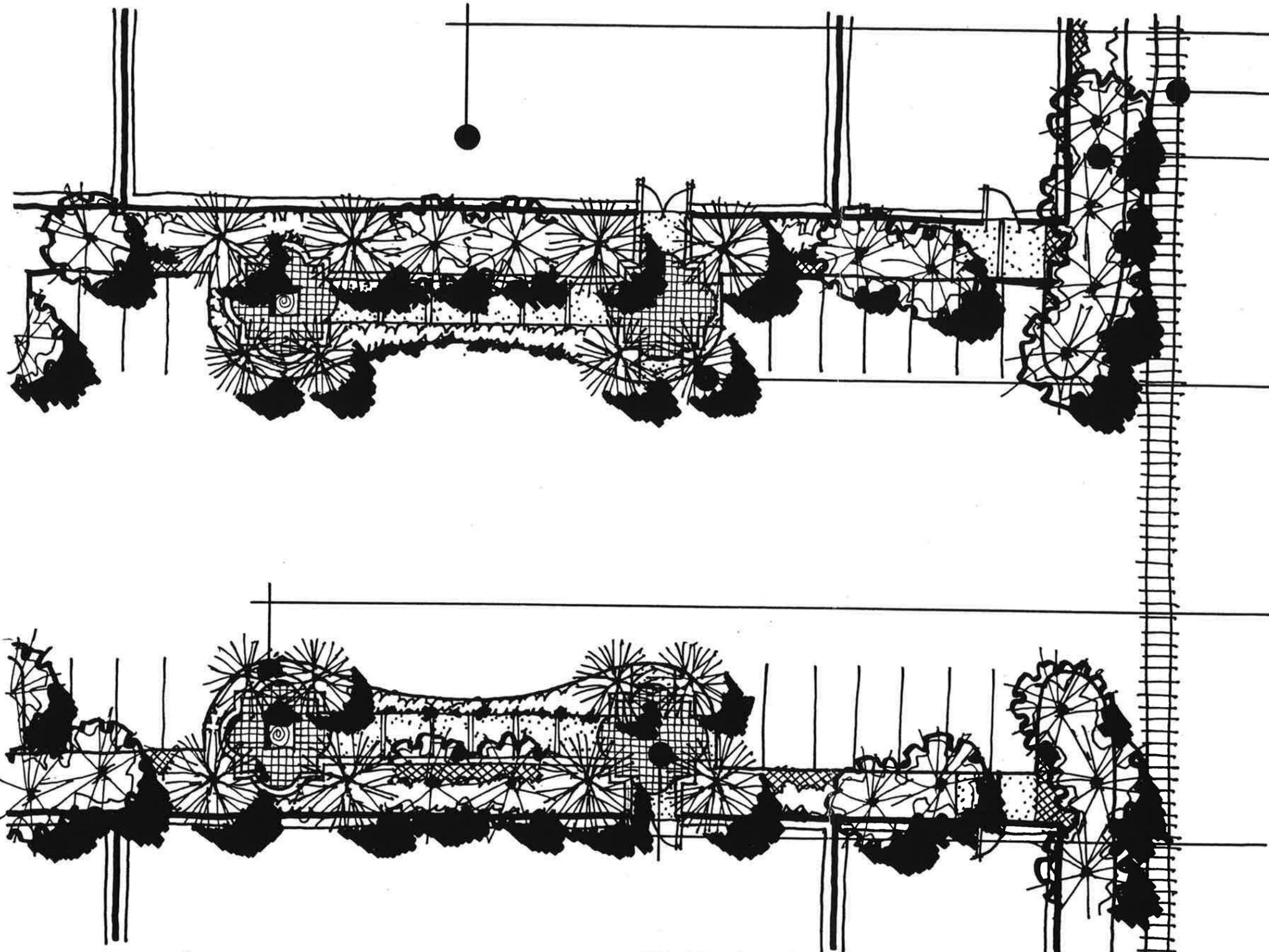
COMMON OUTDOOR PLAZA
@ BUILDINGS 4 and 5

FIGURE 5-3-16



The Keith Companies
Inland Empire Division

H:\31384.000\DWG\SP1\FIG5.1.DWG JULY 1998



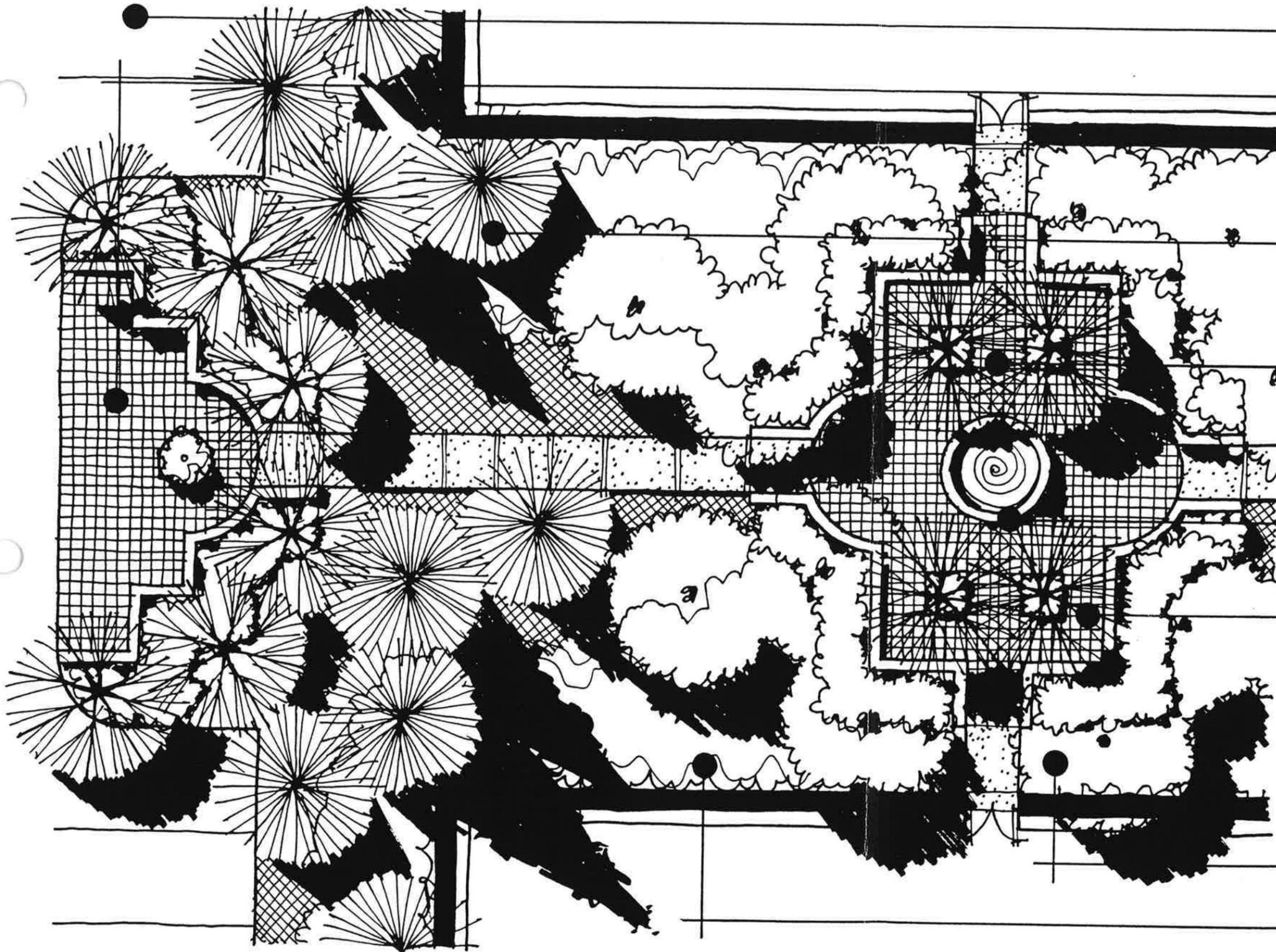
- BUILDING
- RAILROAD SPUR
- INDICATES RHUS LANCEA - AFRICAN SUMAC and PISTACHIA CHINENSIS - CHINESE PISTACHE IN PLANTERS @ BUILDING'S EDGE
- INDICATES LAGERSTROEMIA INDICA - 'WATERMELON RED' - GRAPE MYRTLE ACCENT TREES @ ENTRY/PLAZA EDGE
- ENTRY COURTYARD FOR EMPLOYEE USE W/ 18" HIGH SEATWALLS and SMALL FOUNTAIN
- BUILDING ENTRY W/ ACCENT PAVING and SEATWALLS

PLAN VIEW
SCALE: 1" = 120'

COMMON AREA BETWEEN BUILDINGS 1 & 2, and 2 & 3

Kammeyer & Associates 909-371-2444
BREA INDUSTRIAL
SPECIFIC PLAN

FIGURE 5-3-17



PARKING LOT

PEDESTRIAN ENTRY WITH 18" HIGH SEAT-WALLS and DECORATIVE PAVING

INDICATES PINE and INCENSE CEDAR MASS PLANTINGS

EMPLOYEE COURTYARD w/ 18" HIGH SEATWALLS and DECORATIVE PAVING

SMALL REFLECTION POOL

INDICATES LAGERSTROEMIA INDICA - 'WATERMELON RED' - CRAPE MYRTLES IN PLANTER WELLS

INDICATES GINKGO BILoba - MAIDENHAIR TREE MASSES

INDICATES SHRUB MASS

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BREA INDUSTRIAL
SPECIFIC PLAN

COMMON OUTDOOR PLAZA
BETWEEN BUILDINGS
6 and 7 - 8 and 9

FIGURE 5-3-18



The Keith Companies
Inland Empire Division

H:\31384.000\DWG\SP1\FIG5-1.DWG JULY 1998

6.0 SPECIFIC PLAN IMPLEMENTATION

6.1 PURPOSE AND INTENT

According to Government Code 65451 of the California Planning Law, specific plans shall include a program for implementation. An Implementation Program has been established to meet the goals of the project. Development will be implemented in conformance with the regulations and guidance contained within the Specific Plan document. This section contains the Phasing Plan for the development of the proposed Planning Areas which defines the type, location, intensity and timing of development. This section also contains the programs and comprehensive funding for the projected sequence of development. Implementation of the plan will further be carried out by a method of Precise Development Plan Review and approval as outlined in this section. Other information covered in this section pertains to general administration, subdivision, amendment procedures, and the linkage between these elements. In addition to Precise Development Plan review, the Brea Industrial Specific Plan shall be implemented through the subdivision process. The subdivision process will allow for the creation of lots as tentative parcel or tentative tract maps which will allow for implementation of project phasing. A portion or portions of the Brea Industrial Specific plan area may be separately financed.

6.1.1 Relationship to the Zoning Code

The Brea Industrial Specific Plan will be adopted by resolution of the Brea City Council. Adoption of the Specific Plan defines land use and development standards for the project area which shall supercede current zoning regulations.

This Specific Plan augments the development regulations and standards of the Brea Zoning Code. When an issue, condition or situation occurs which is not covered or provided for in this Specific Plan, the regulations of the Zoning Code that are most applicable to the issue, condition or situation shall apply. In the event that the provisions of the Specific Plan are in conflict with the Zoning Code, the provisions of the Specific Plan shall prevail.

The following changes to explicit provisions in the Specific Plan may be made administratively by the Development Services Department, subject to appeal to the Planning Commission and, subsequently, the City Council:

- A. Changes in the location, number, and size of Planning Areas/Buildings within the General Industrial or the Light Industrial project areas, provided that the maximum allowable lot coverage within the respective General Industrial and light Industrial land use designations is not exceeded.
- B. The addition of new information to the Specific Plan maps or text that does not change the effect of any regulations adopted by ordinance or resolution.

- C. Changes to community infrastructure, such as drainage, water, and sewer systems which do not have the effect of increasing or decreasing development capacity in the Specific Plan area, nor change the concepts of the Plan.
- D. The determination that a use be allowed which is not specifically listed as permitted, but which may be determined to be similar in nature to those uses explicitly listed as permitted.

In accordance with the California Government Code Sections 65454, Specific Plans shall be prepared, adopted and amended in the same manner as General Plans, except that Specific Plans may be adopted by resolution or by ordinance. This plan may be amended as necessary in the same manner it was adopted, by resolution. Said amendment or amendments shall not require a concurrent General Plan Amendment unless it is determined by City staff that the proposed amendment would substantially affect the General Plan goals, policies, or programs. An environmental assessment form shall accompany the proposed amendment, but it is presumed that the Environmental Impact Report (EIR) approved for the project area includes all future development for the Specific Plan. If further environmental documentation is required, in special cases, it will be a focused analysis and action, as documented in Section 15162 and 15182 of the California Environmental Quality Act (CEQA) Guidelines.

6.2 PHASING PLAN

The Brea Industrial Specific Plan is proposed to be developed in three (3) phases. Within the various phases of development, phased development of the individual land uses as specified in the land use plan may occur. The Phasing Plan is designed to provide an adequate level of infrastructure to accommodate development of the proposed land uses within each phase.

It is the primary intention of the Phasing Plan to relate infrastructure requirements to proposed development. While a sequence is implied, there is nothing in this plan to preclude a different order of development, or even a different combination of sub-phases, so long as the related infrastructure is adequately in place. "Full width" improvements to the Mariner Street extension from Arovista Avenue to Puente Street shall be constructed as part of the first phase of development, and shall be open to through traffic before any building is occupied. The Specific Plan provides for flexibility because the actual sequence of development may be affected by numerous factors including market trends, demand for products and available financing. The proposed phasing is conceptual in nature and represents the best projections for project phasing at this stage of development. The following conceptual alternatives reflect potential phasing scenarios in a logical and systematic approach.

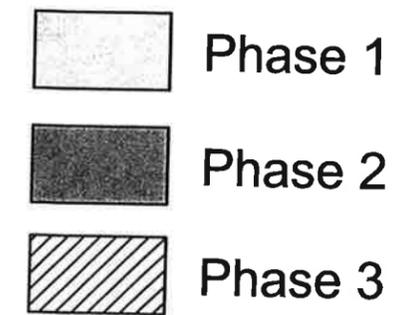
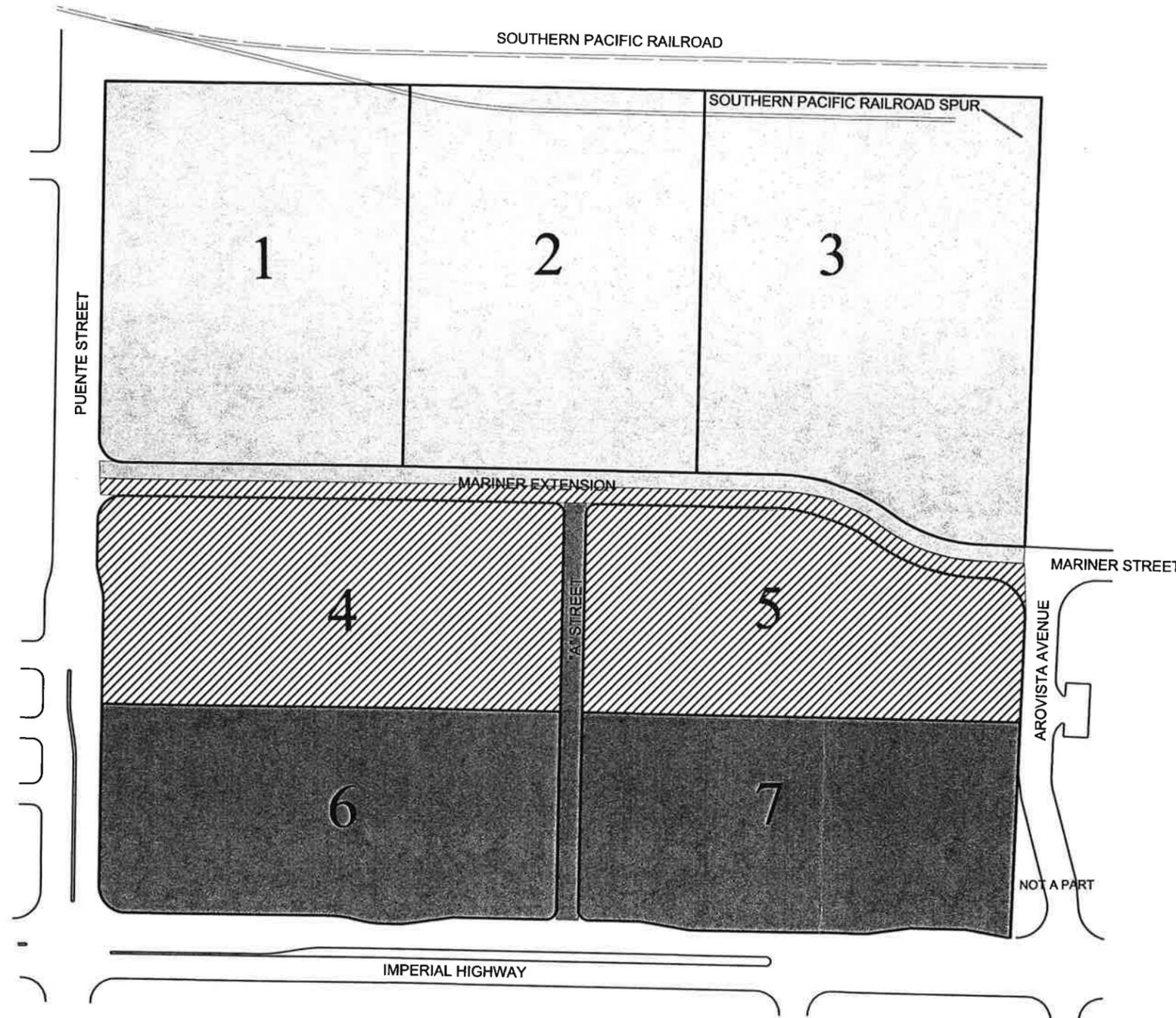
Alternative 1

Under this scenario, as depicted in Exhibit 6-1, Phase 1 will develop the northern portion of the Specific Plan area. This includes Planning Areas 1 through 3 and full width improvements to the Mariner Street extension. Phase 2 will develop the southern portion of the Specific Plan area. This includes Planning Areas 6 and 7, and improvements to "A" Street. Phase 3 will develop the central portion of the Specific Plan area. This includes Planning Areas 4 and 5.

Alternative 2

Under this scenario, as depicted in Exhibit 6-2, Phase 1 will develop the southern portion of the Specific Plan area. This includes Planning Areas 6 and 7, full width improvements to the Mariner Street extension and improvements to "A" Street. Phase 2 will develop the northern portion of the Specific Plan area. This includes Planning Areas 1 through 3. Phase 3 will develop the central portion of the Specific Plan area. This includes Planning Areas 4 and 5.

PHASING PLAN ALTERNATIVE 1

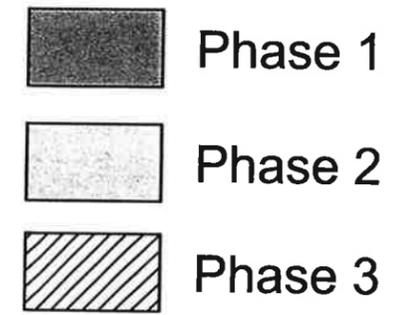
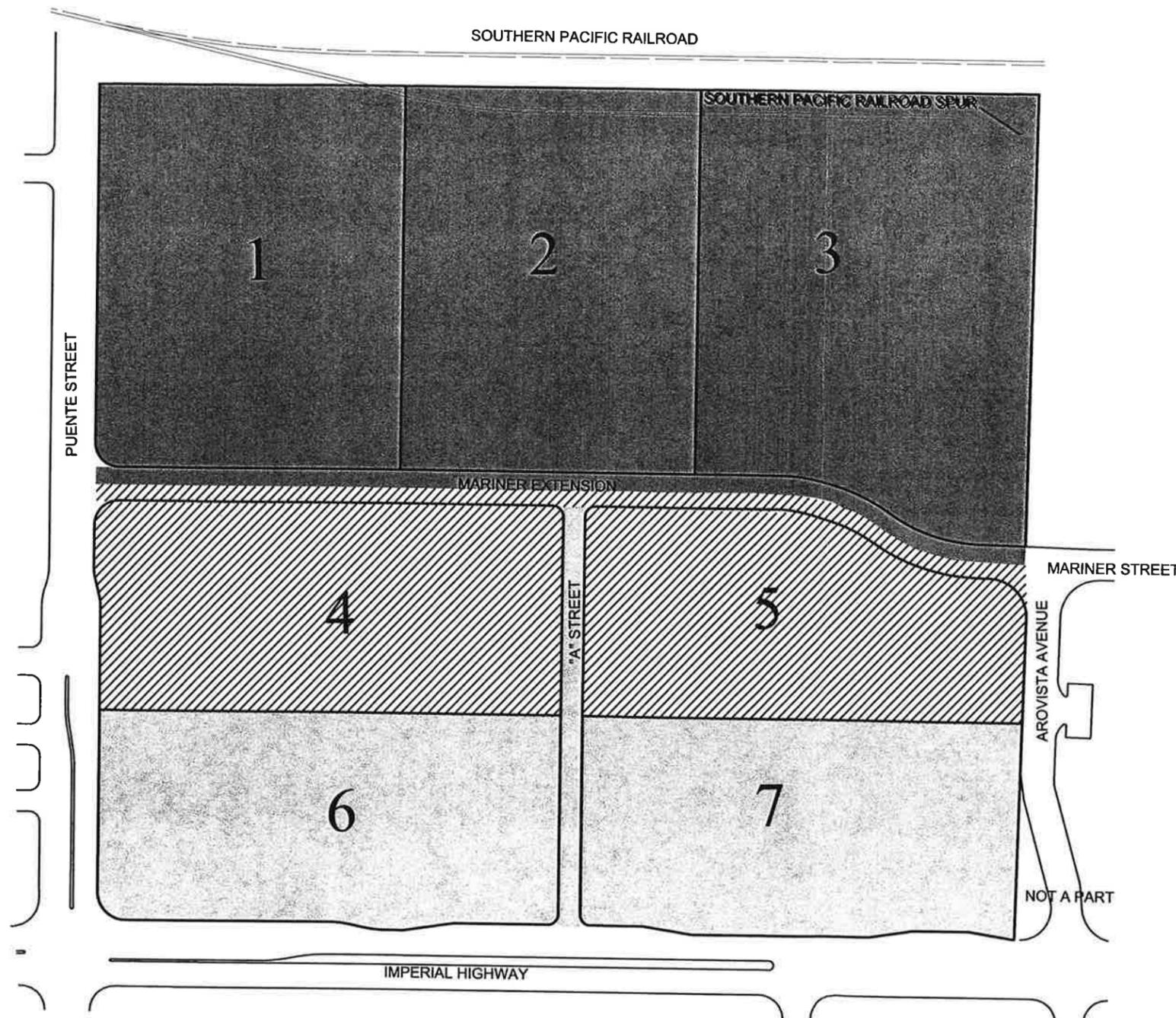


BREA INDUSTRIAL
SPECIFIC PLAN



FIGURE 6-1A

PHASING PLAN ALTERNATIVE 2



BREA INDUSTRIAL
SPECIFIC PLAN

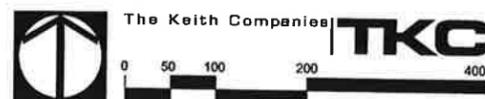


FIGURE 6-1B

6.3 SPECIFIC PLAN PRECISE DEVELOPMENT PLAN REVIEW

The Brea Industrial Specific Plan shall be implemented through a method of Precise Development Plan review, and individual projects within the Specific Plan area must meet City of Brea Precise Development Plan Review requirements. A Precise Development Plan review shall be required for all development within the Specific Plan area requiring a building permit. Tentative parcel and tract maps may be processed independent of the Precise Development Plan review procedures, in accordance with the Brea Subdivision Ordinance. Precise Development Plan review will not be required for interior alterations where there is not an increase to square footage or use intensification. For permitted uses subject to a Precise Development Plan pursuant to the provisions of Section 3.0 of this Specific Plan, the procedures specified in Section 20.260 of the Brea Zoning Code shall apply. Precise Development Plans shall require approval by the Planning Commission.

All proposed projects within the Specific Plan area shall be required to have an approved Precise Development Plan prior to issuance of building permits or concurrent with subdivisions, conditional use permits or any other permit for the property. The Precise Development Plan review procedure is necessary for the following reasons:

- A. To ensure consistency with the Specific Plan and all implementing ordinances.
- B. To promote the highest contemporary standards of site design.
- C. To adapt to specific or special development conditions that may occur while continuing to implement the Specific Plan and conform development to the implementing ordinances.
- D. To facilitate complete documentation of land use entitlements authorized and conditions pertinent thereto.
- E. To adapt to substantial changes that may occur with respect to the circumstances under which the project is developed.

To facilitate implementation, this Specific Plan has been adopted by ordinance. This ordinance establishes that all development of the Specific Plan area will be governed, in order, by (1) this Specific Plan and (2) all other general ordinances of the City relating to the development of property within the City's jurisdiction, including the City's Subdivision Ordinance No. 569 and Zoning Code.

6.3.1 Tract Map/Parcel Approval

All subdivision maps of any type (e.g. tentative or final, vesting or nonvesting, tract or parcel) shall be submitted, reviewed and approved in accordance with the City of Brea Subdivision Ordinance No. 569 and the California Subdivision Map Act. Where the provisions or procedures of Subdivision Ordinance No. 569 conflict with the provisions of this Specific Plan, the Specific Plan shall apply.

A large lot tentative parcel map(s) may be prepared for financing or conveyance purposes and subject to further subdivision prior to construction or development, as permitted by Section 820, of the City's Subdivision Ordinance No. 569. Said subdivision shall be subject to review and approval in accordance with Section 310 of the City' Subdivision Ordinance.

6.3.2 Revisions to the Precise Development Plan

After the City of Brea has approved a Precise Development Plan, minor changes to the size, location, and design of any uses or structures may be approved by the Development Services Director pursuant to the Brea Industrial Specific Plan. If the Development Services Director determines that the proposed revisions are in substantial conformance with the provisions of the Brea Industrial Specific Plan and the general intent of the approved Precise Development Plan, the revised Precise Development Plan shall be approved by the Development Services Director. The decision of the Development Services Director shall be final, unless appealed to the Planning Commission within ten (10) days from the date of such decision. The Development Services Director may exempt the following revisions or changes from additional Planning Commission review and approval:

- A. All interior changes including alterations to the size of commercial buildings that do not result in an increase of intensity.
- B. Minor variations in architectural elevations and footprints.
- C. Minor adjustments to site plan and/or landscape plan as a result of engineering refinements and/or City's review and revisions.

6.3.3 Site Plans/Conditional Use Permits/Variances

Except where this Specific Plan provides otherwise, the provisions of the City's Zoning Code outlining "Administration and Procedures" (Division III) shall apply.

6.4 GENERAL ADMINISTRATION

The Brea Industrial Specific Plan shall be administered and enforced by the City of Brea Development Services Department in accordance with the provisions of the City of Brea Zoning Code.

Certain changes to explicit provisions in the Specific Plan may be made administratively by the Development Services Director, subject to appeal to the Planning Commission and, subsequently, the City Council, including A., B. and C. as follows:

- A. The addition of new information to the Specific Plan maps or text that does not change the effect of any regulations or guidelines.
- B. Changes to the community infrastructure, such as drainage, water and sewer systems which do not have the effect of increasing or decreasing development capacity in the Specific Plan area, nor change the concepts of the Plan.
- C. The determination that a use be allowed which is not specifically listed as permitted, but which may be determined to be similar in nature to those uses explicitly listed as permitted.

6.5 SUBDIVISION

The City of Brea Subdivision Ordinance shall apply to all development within the Specific Plan area required to obtain approval of subdivision maps where properties are to be separately financed, sold, leased or otherwise conveyed. The subdivision process will allow for the creation of lots as parcel or tract maps which will allow for implementation of the phasing plan. The review and approval of subdivision maps would set forth the various conditions necessary to ensure the improvement of streets, utilities, drainage features, and other service requirements specified by the City.

6.6 AMENDMENT PROCEDURES

In accordance with the California Government Code Sections 65453-65454, specific plans shall be prepared, adopted and amended in the same manner as general plans, except that specific plans may be adopted by resolution or by ordinance.

This Specific Plan may be amended as necessary in the same manner it was adopted, by ordinance. Said amendment or amendments shall not require a concurrent general plan amendment unless it is determined by City staff that the proposed amendment would substantially affect the general plan goals, objectives, policies and programs.

6.7 SPECIFIC PLAN PROGRAMS/COMPREHENSIVE FUNDING PLAN

The ability to fund public facilities necessary for support of development is one of the most challenging aspects of the planning and development process. The overall approach proposed in the Brea Industrial Specific Plan is to use one or more of the basic funding approaches indicated to phase logical and affordable increments of the Public Facilities and Services in conjunction with appropriate components of development. Although the following list is not considered all-inclusive, some of the most common financing methods are discussed below.

6.7.1 Conventional Financing

Conventional financing has historically been the norm in California development. This has involved developers, landowners, and homebuyers paying for the facilities through exactions and fees. The overall purpose was to make development pay for itself. Proposition 13 reduced the ability of conventional financing to provide adequate funding. It removed a big piece of the property tax that used to produce local revenues from new developments. This money had been used to service the City's facilities and provide protective services. Interest rates on conventional loans for facilities have become too high to make it feasible in many cases.

Due to current interest rates and the amount of money that must be financed, the main investors who can afford to install the needed infrastructure are those with access to large assets, such as banks, savings and loans and insurance companies. Cities now must find ways to finance the facilities and services, due to the limitations on property tax revenue. These costs, which are increasing all the time, are frequently too great for the developer or jurisdiction. Therefore, other creative financing mechanisms may come into play.

In the past, bond issues were the simplest and most often utilized method of raising large amounts of capital. Because of changing conditions in the financial marketplace, and increased legislation limiting the bonding power of many local governments, the role of the traditional general obligation bond has been substantially reduced. Other bonding alternatives include:

- Revenue Bond
- Lease-Revenue Bond
- Zero Coupon Bond
- Stepped Coupon Bond

These techniques have historically been used for redevelopment and therefore would probably have limited application in the Specific Plan development.

6.7.2 Assessment Districts

The Municipal Improvement Act of 1913 (Streets and Highways Code Section 10000 *et. seq.*) is a procedural act which provides for the formation of a special assessment district, the levy of an assessment and the creation of a lien against the property, but does not, in itself, contain provisions for the issuance of bonds. (For this reason, bonds are issued under the 1911 Act or 1915 Act discussed below.) As assessment district is created for acquisition and construction of a wide variety of public services, including street improvements, public utilities and community-wide recreation amenities.

Under the 1913 Act, a proposed assessment and assessment diagram are prepared before any construction work is done. The amount of the assessment levied against each property is based upon an engineer's report and notices are sent to property owners. A public hearing is held, both upon the project and the amount of the proposed assessment to be levied. At the conclusion of the public hearing the governing body may confirm the assessments. If confirmed, a lien is created against each parcel and the assessments are properly recorded. The property owners are mailed notices of each parcel's exact confirmed assessments and they have 30 days to pay a part or all of the assessment in cash. Upon conclusion of the cash collection period, all unpaid assessments are accumulated, a bond issue is structured and bonds are sold. The bonds may be issued pursuant to either the Improvement Act of 1911 (Streets and Highways Code Section 5000 *et. seq.*) or the Improvement Bond Act of 1915 (Streets and Highways Code Section 8500 *et. seq.*).

Each bond issued under the 1911 Act constitutes a direct lien against a specific piece of property. This lien is on a parity with general taxes and takes priority over any private lien such as a deed of trust, mortgage or attachment. In the event the property owner fails to pay an installment on or prior to its delinquency date, penalties accrue against the delinquent amount, and such penalties are paid to the bondholder upon redemption of the delinquent installment or installments. If redemption of a delinquency is not made, the bondholder may institute the foreclosure action.

Under the 1915 Act, individual bonds are not issued to represent individual assessments. Bonds equaling the total unpaid assessment are issued in even denominations and a portion of the bonds are due each year for the life of the issue which will result in approximately equal annual principal and interest payments. The assessment liens securing the bonds are payable in installments which, in the aggregate, conform to the principal and interest payments on the bonds. Assessment installments appearing on the regular property tax bill are collected in the same time and in the same manner as property taxes, and bear the same penalties for delinquency including the sale of the property at public auction. A special reserve fund is available from which to make payments to the bondholder of any delinquent assessments which might occur, and foreclosure proceedings, similar to those in the 1911 Act, are also available. These liens are also superior to other deeds of trust and similar liens.

7.0 APPENDIX

7.1 GENERAL PLAN CONSISTENCY

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450 - 65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Specific plans must demonstrate consistency in regulations, guidelines and programs with the goals and policies set forth in the general plan.

The City of Brea General Plan contains the following elements: Land Use, Circulation, Housing, Open Space and Conservation, Parks and Recreation, Safety, Noise, Growth Management, and Historic Resources.

A general plan amendment to the Land Use Element is being processed in conjunction with the Brea Industrial Specific Plan. This Specific Plan has been prepared in conformance with the goals and policies of the Brea General Plan, as proposed to be amended. This section of the Specific Plan Appendix includes a discussion of the relevant goals and policies of pertinent General Plan Elements.

This project is located in the City of Brea, Orange County, California. The proposed General Plan designation for the site is Specific Plan (SP). However, the proposed land uses as outlined in the Brea Industrial Specific Plan are consistent with the current General Plan designation of Light/General Industrial. This section documents the consistency with the Brea General Plan.

7.1.1 Land Use Element

This section describes the Land Use Element as it relates to the Brea Industrial Specific Plan.

Goal: Achieve a rural-suburban city with land uses which meet the needs of Brea's neighborhoods, the entire community and regional employment, shopping and housing needs.

Project Implementation:

The proposed project consists of a Specific Plan which includes a maximum of 619,250square feet of industrial, office and warehousing/distribution development uses. This project will provide regional employment opportunities.

Goal: Provide for a balanced land use distribution in Brea's neighborhoods, including single-family and multi-family housing; convenience and neighborhood shopping centers; park and recreation uses; and other appropriate uses.

Project Implementation:

The Brea Industrial Specific Plan includes industrial land uses that are consistent with the existing surrounding land uses in the project area. The proposed land uses are consistent with the existing General Plan designation of the property.

Goal: Provide for land use development and employment expansion in the western and eastern industrial sectors of the City.

Project Implementation:

The Brea Industrial Specific Plan provides an expansion to the existing base of industrial land uses in the western industrial sector of the City.

Policy: To retain the land uses which have been designated for most of the remaining areas of the City, including the residential, commercial and industrial sectors.

Project Implementation:

The Brea Industrial Specific Plan will retain and complement the land uses in and around the project site.

7.1.2 Circulation Element

This section describes the Circulation Element as it relates to the Brea Industrial Specific Plan.

Goal: To promote the safe and efficient movement of people and goods.

Project Implementation:

The Brea Industrial Specific Plan promotes the safe and efficient movement of people and goods within and adjacent to the Specific Plan area by creating a safe circulation system for people, automobiles, trucks and trains within and adjacent to the Specific Plan area. Safety improvements include pedestrian walkways, automobile and truck driveways and parking areas, an extension of Mariner Street west to Puente Street, signal improvements, and utilization of the existing railroad spur on-site.

Goal: To expand the circulation system to accommodate planned development.

Project Implementation:

The existing circulation system adjacent to the project site will be expanded in accordance with Exhibit 3-3 of the Specific Plan and through implementation of mitigation measures recommended in the project's Environmental Impact Report (EIR). Expansion of the circulation system will include the extension of Mariner Street west to Puente Avenue.

Goal: To alleviate existing circulation system deficiencies.

Project Implementation:

Implementation of the Brea Industrial Specific Plan will not interfere with the City's goal to alleviate existing circulation system deficiencies.

Goal: To encourage alternate modes of transportation.

Project Implementation:

The Brea Industrial Specific Plan may utilize the existing railroad spur on the northwest portion of the site for movement of goods to and from the project site.

Goal: To increase the operational efficiency of the circulation system at both the local and regional levels.

Project Implementation:

The Brea Industrial Specific Plan will incorporate circulation system mitigation measures as recommended by the EIR to assist in increasing the operational efficiency of the circulation system. The project will also construct the extension of Mariner Street west to Puente Street.

Policy: To coordinate with the Orange County Transit District to respond to service demands as new development occurs.

Project Implementation:

The project applicant will coordinate with the Orange County Transportation Authority as required by mitigation measures documented in the EIR for the Specific Plan project.

Policy: To encourage new commercial development to employ transportation system management strategies.

Project Implementation:

The project applicant will implement transportation system management strategies as required by mitigation measures outlined in the EIR for the Specific Plan project.

Policy: To require improved street access for all development.

Project Implementation:

The Specific Plan Circulation Plan responds to the needs of the development and accommodates projected future traffic generated from the Specific Plan area and surrounding land uses. The Specific Plan provides sufficient access points to the project site from Imperial Highway and Puente Street, as well as the extension of Mariner Street west through the site to Puente Street. The Specific Plan will implement all mitigation measures for transportation impacts documented in the project's EIR.

Policy: To encourage provisions for bus turnouts in specific development projects.

Project Implementation:

The proposed development will provide for bus turnouts as required by mitigation measures documented in the project's EIR.

Policy: To promote other means of achieving circulation and transportation efficiencies such as ride-sharing and staggered hours.

Project Implementation:

The proposed development will incorporate mitigation measures required by the project's EIR pertaining to circulation and transportation efficiencies. It is the intent of this Specific Plan to comply with the requirements of the City's Transportation Demand Management Ordinance.

Policy: To evaluate, in conjunction with the Orange County Transportation Commission, the feasibility of Imperial Highway being designated a "super street."

Project Implementation:

The Brea Industrial Specific Plan will not interfere with the City's ability to work with the Orange County Transportation Commission on a super street designation for Imperial Highway, which has already occurred. Project related improvements to Imperial Highway shall be consistent with the super street designation.

7.1.3 Safety Element

This section describes the Safety Element as it relates to the Brea Industrial Specific Plan.

Goal: Provide a safe and nonhazardous environment for the Brea community.

Project Implementation:

Phase I and Phase II Environmental Assessments will be performed for the site which identify all contaminated areas located on the subject property. If required, a Remedial Action Plan will be prepared to identify the measures necessary to fully remediate any contaminated soils and/or groundwater. In addition, safety concerns will be addressed in the project' EIR.

Goal: Minimize the potential for loss of life and property in the event of a seismic event.

Project Implementation:

The City of Brea Building Code provides seismic requirements to minimize any structural damage during a seismic event. The Uniform Building Code requirements shall be incorporated into development of the Brea Industrial Specific Plan.

Policy: To identify seismic and other geologic hazards.

Project Implementation:

The Brea Specific Plan area does not contain any seismic or other geologic hazards.

Policy: To establish requirements for adequate construction in geologically hazardous areas.

Project Implementation:

The Brea Industrial Specific Plan area is not located within a geologically hazardous area.

Policy: To ensure continuity of vital services following an earthquake.

Project Implementation:

The Brea Industrial Specific Plan will not interfere with the City's ability to ensure the provision of vital services following an earthquake.

Policy: To participate in regional emergency response planning.

Project Implementation:

The Brea Industrial Specific Plan will not interfere with the City's efforts towards regional emergency response planning.

Policy: To establish evacuation routes on a site specific basis, pursuant to Fire Department policies.

Project Implementation:

The Brea Industrial Specific Plan will conform to the requirements of any established evacuation routes.

Policy: To require a road with two 12-foot lanes (plus parking) for emergency access per requirements of the Brea Fire Department.

Project Implementation

All roadways internal to the Brea Industrial Specific Plan area are proposed to contain lanes with widths of 12 feet, plus parking to meet the minimum requirements of the Brea Fire Department.

7.1.4 Noise Element

This section describes the Noise Element as it relates to the Brea Industrial Specific Plan.

Goal: To minimize noise impacts to the people who live and work in Brea.

Project Implementation:

The Brea Industrial Specific Plan utilizes landscaped setbacks and walls to minimize noise impacts from adjacent arterial highways.

Goal: To control noise in Brea for the protection of the health and well being of its current and future citizens.

Project Implementation:

The Brea Industrial Specific Plan does not propose uses which generate high noise levels. Any impacts resulting from noise will be mitigated as outlined in the project EIR.

Goal: To maintain or reduce noise levels in noise sensitive areas.

Project Implementation:

The Brea Industrial Specific Plan will implement the mitigation measures recommended in the EIR to minimize noise impacts.

Policy: To reduce transportation noise by imposing traffic restrictions where necessary.

Project Implementation:

The Brea Industrial Specific Plan will not interfere with the City's policy to reduce transportation noise.

Policy: To identify potential land use conflicts and to determine where acoustical analysis/mitigation studies will be necessary through the periodic use of noise contour maps and noise impact tables.

Project Implementation:

The Brea Industrial Specific Plan EIR recommendations for noise mitigation will be implemented.

Policy: To evaluate the impacts of specific projects in the context of the cumulative noise impacts presented in the noise contour maps and noise impact tables.

Project Implementation

The Brea Industrial Specific Plan EIR will contain a noise analysis, including recommended mitigation measures, to minimize noise impacts.

Policy: To require project applicants to reduce or buffer noise generated by a proposed development if it would otherwise create an unsatisfactory noise environment for adjacent properties.

Project Implementation:

The Brea Industrial Specific Plan guidelines and development regulations provide the requirements on setbacks, landscaping, walls, etc., to ensure that adjacent land uses will not impact one another and will be adequately screened and/or buffered for noise reduction where necessary.

7.1.5 Growth Management Element

This section describes the Growth Management Element as it relates to the Brea Industrial Specific Plan.

Goal: To reduce traffic congestion.

Project Implementation:

The EIR for the Brea Industrial Specific Plan will contain a traffic analysis, including mitigation measures, to minimize the traffic impacts of the proposed project and reduce traffic congestion.

Goal: To ensure that adequate transportation facilities are provided for existing and future residents of the City.

Project Implementation:

The EIR for the Brea Industrial Specific Plan will contain a traffic analysis, including mitigation measures, to ensure the availability of adequate transportation facilities for potential circulation system impacts that result from the proposed project.

Policy: The Level of Service standard for traffic circulation in the City shall be LOS "D" for intersections under the sole control of the City.

Project Implementation:

The Brea Industrial Specific Plan shall comply with the provisions of the County Congestion Management Program and Growth Management Plan.

Policy: In order to achieve LOS "D", it shall be the City's policy that within three years of the issuance of the first building permit for a development project, the necessary improvements to the transportation facilities identified as adversely impacted, shall be completed.

Project Implementation:

The Brea Industrial Specific Plan shall comply with the City's policy regarding achievement of LOS "D", where applicable.

Policy: All new development shall pay for or implement its fair share of the street improvement costs associated with that development including regional traffic mitigation.

Project Implementation:

The EIR for the Brea Industrial Specific Plan will contain mitigation measures to ensure that the project pay for or implement its fair share of street improvements.

Policy: New development in the City shall be required to establish a Development Phasing Program which phases approval of development by linking issuance of building permits for the appropriate portion of the development plan (by phase and/or number of units) to roadway improvements required to achieve the appropriate Level of Service in one or more of the following three ways determined during the development approval process: 1) linkage to construction of the designated improvement(s) by the developer; 2) linkage to construction of the designated improvement(s) by others; or 3) linkage to payment of the project's share of the funding for designated improvement(s) at a certain time designated by the City. The phasing plan shall include an overall build out plan which can demonstrate the ability of the infrastructure to support the planned development.

Project Implementation:

The Brea Industrial Specific Plan will establish all required roadway improvements prior to completion of the proposed project.

Policy: The City shall monitor the implementation of the Development Phasing Program for each of its new development projects on an annual basis and prepare a report which indicates the status of development approval and the required traffic improvements and the relationship between them.

Project Implementation:

The Brea Industrial Specific Plan does not interfere with the City's ability to monitor the project's phasing.

Policy: The City shall participate in Inter-jurisdictional Planning Forum(s) as established by the Regional Advisory Planning Council.

Project Implementation:

The Brea Industrial Specific Plan does not interfere with the City's policy on participation in Inter-jurisdictional Planning Forum(s).

Policy: A seven year Capital Improvement Program shall be adopted and maintained in conformance with the provisions of Measure M for the purpose of maintaining the Levels of Service established in this Element.

Project Implementation:

The Brea Industrial Specific Plan does not interfere with the adoption or maintenance of the City's Capital Improvement Program.

Policy: Recognizing its role as one of North Orange County's major employment centers, and the fact that the City is about 98% built out, it is the City's policy to strive toward achieving a balance of land use whereby residential, commercial, industrial and public uses are proportionally balanced.

Project Implementation:

The Brea Industrial Specific Plan proposes to develop the subject property with uses consistent with the City's General Plan designation for the site.

Policy: The City shall promote traffic reduction strategies through the measures adopted by its Transportation Demand Management Ordinance.

Project Implementation:

The Brea Industrial Specific Plan shall comply with the City's Transportation Demand Management Ordinance and with mitigation measures as listed in the project's EIR relating to transportation.

7.1.6 Historic Resources Element

This section describes the Historic Resources Element as it relates to the Brea Industrial Specific Plan.

Goal: Preserve historically significant landmarks and/or sites, individual structures, objects, and neighborhoods and encourage appropriate rehabilitation.

Project Implementation:

The EIR for the Brea Industrial Specific Plan will contain a Cultural Resources analysis, including mitigation measures, to avoid any impacts to cultural resources as a result of the proposed project.

Policy: Structures eligible for listing in the National Register of Historic Places and/or the State Historic Landmarks, the County, and/or the local historic register should not be demolished until all reasonable and feasible alternatives for saving them have been explored.

Project Implementation:

The Brea Industrial Specific Plan area does not contain any structures eligible for listing in the National Register of Historic Places and/or the State Historic Landmarks, the County, and/or the local historic register.

Policy: New development in proximity to designated historic structures and neighborhoods should be designed to ensure compatibility with the characteristics of the designated historic structures.

Project Implementation:

No historic structures exist in the immediate project vicinity.

7.2 FINDINGS

1. The proposed Specific Plan is consistent in intensity and character with the City's adopted General Plan, as amended.
2. Reasonable alternatives to the plan and their implications have been considered.
3. The scope and depth of environmental and policy analysis are commensurate with the level of detail contained in the plan and the specificity of land use entitlement its adoption authorizes.
4. Implementation of the Specific Plan ensures that concerns identified at this level of planning are resolved as part of the more detailed Precise Development Plan review, which must be completed before development may proceed.
5. Administration of the plan is thoroughly integrated into the City's development processing system.
6. All subjects required in a Specific Plan by the California Government Code and applicable City ordinances are appropriately and adequately covered.
7. Adequate time and opportunities have been afforded interested organizations and members of the public to comment on, or propose changes to, the plan if they so desired.
8. The plan and its policies will be used to guide development in the City and shape all subsequent land use entitlements for the Specific Plan area.

7.3 DEFINITIONS

- Arterial Roads** - Shall mean those roadways depicted on the City of Brea's Master Plan of Arterial Highways.
- Brea Industrial Specific Plan** - Shall mean those parcels, as depicted on Exhibit 2-2, Vicinity Map, and legally described in Section 7.4.
- Gross Acre** - Shall mean the entire land area within the Specific Plan boundary measured to the centerline of adjacent streets and including collector and local roads internal to the Specific Plan.
- Net Acre** - Shall mean the entire land area within Specific Plan boundary measured to the right-of-way of adjacent streets and including collector and local roads internal to the Specific Plan.
- Planning Area** - Shall mean a geographic area within the Specific Plan area, as depicted on Exhibit 3-1, Land Use Plan and Planning Areas, and referenced on Table 3-1, Land Use Plan Summary, that contains a land use designation and acreage.

7.4 LEGAL DESCRIPTION

The property is that certain real property located in the City of Brea, County of Orange, State of California, more particularly described as follows:

That portion of the South one-half of the Southeast Quarter of Section 10, Township 3 South, Range 10 West, in the Rancho San Juan Cajon de Santa Ana, as shown on a map filed in Book 12, Pages 47, 48 and 49 of Records of Survey, in the office of the County Recorder of Orange County, California, and in Book 51, Page 7 of Miscellaneous Maps, Records of said Orange County, described as follows:

Beginning at a point on the south line of said Section 10 North 88° 28' 00" West, 1,284.15 feet from the southeast corner of said Section 10;

Thence North 88° 28' 00" West 509.49 feet along said south line;

Thence North 2° 54' 40" East, 1,263.72 feet to the south line of the land conveyed to H.E. Huntington, Trustee, by deed recorded May 13, 1907 in Book 155, Page 358 of Deeds, Records of said Orange County;

Thence South 89° 19' 40" East, 456.89 feet to an angle point on the south line of said land conveyed to Huntington;

Thence continuing along said south line North 89° 20' 55" East, 52.38 feet to a line drawn North 2° 53' 15" East from the Point of Beginning;

Thence South 2° 53' 15" West, 1,272.58 feet to the Point of Beginning.

Excepting therefrom that portion included within the land described in deed to the City of Brea recorded July 26, 1977 in Book 12307, Page 24 of Official Records of said Orange County.